

# PLANNING JUSTIFICATION REPORT

14086 Yonge Street, Aurora, ON



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## 1.0 Introduction

JKO Planning Services Inc. has been retained by Esmail Zamani owner of 14086 Yonge Street, Aurora, ON, to prepare a Planning Justification Report that examines the pertinent policies applicable to the subject property. This report will provide rationale for the development of a 20 residential condominium townhouse project within three (3) development blocks, to be facilitated via a private condominium laneway providing direct access along the west side of Yonge Street.

The subject property is legally described as:

PT LT 1 PL 132 AS IN KI25783 EXCEPT R679438; AURORA

This development proposal provides for a total of 20 residential condominium townhomes within three (3) development blocks. This application attempts to address staff suggestions and requirements raised via the Pre-Submission Meeting held on October 18, 2023, in conjunction with subsequent discussions with Town Staff.

The analysis presented in this report is founded on the following work and policy documents:

- A review of all available background reports and information;
- A detailed examination of existing development constraints and issues (i.e., transportation and planning);
- Provincial Policy Statement 2020;
- Growth Plan for the Greater Golden Horseshoe 2020;
- Oak Ridges Moraine Conservation Plan;
- York Region Official Plan 2022;
- Town of Aurora Official Plan 2010 (including OPA 34 and OPA 48); and
- Town of Aurora Zoning By-law No. 6000-17.

## Historical Background

This proposal is a revision from the previous applications for the subject lands that sought to create five (5) single-detached dwellings on parcels of tied land fronting a common element condominium road connecting to the west side of Yonge Street. It is noted that the revised proposal encompasses the same land area as the original proposal.

A Pre-Consultation Meeting was held on January 16, 2020, and revised comments were received from the Town of Aurora on April 24, 2020, based on the five (5) dwelling unit concept. The original proposal was submitted on October 29, 2021, and deemed complete on November 25, 2021.

The original proposal included a complete submission with the required professional studies, including an Urban Design Brief, Natural Heritage Evaluation, Phosphorus Assessment, Preliminary Hydrogeological Assessment, Functional Servicing and Stormwater Management Report, Preliminary Geotechnical Investigation, Phase 1 Environmental Site Assessment, Environmental Noise Assessment, and a Stage 1 & 2 Archaeological Assessment. This original submission was then formally withdrawn after discussions between the owner and Town Staff.

As outlined above, the revised proposal is being submitted following a Pre-Consultation meeting with the Town of Aurora on October 18, 2023.

## 2.0 Site Characteristics

The subject property is located along Yonge Street in Aurora, Ontario, approximately 1 kilometer north of the intersection of Yonge Street and Bloomington Road. The site is rectangular in shape with an approximate area of 3,951.45 square metres and a lot frontage of 25.23 metres along the west side of Yonge Street.

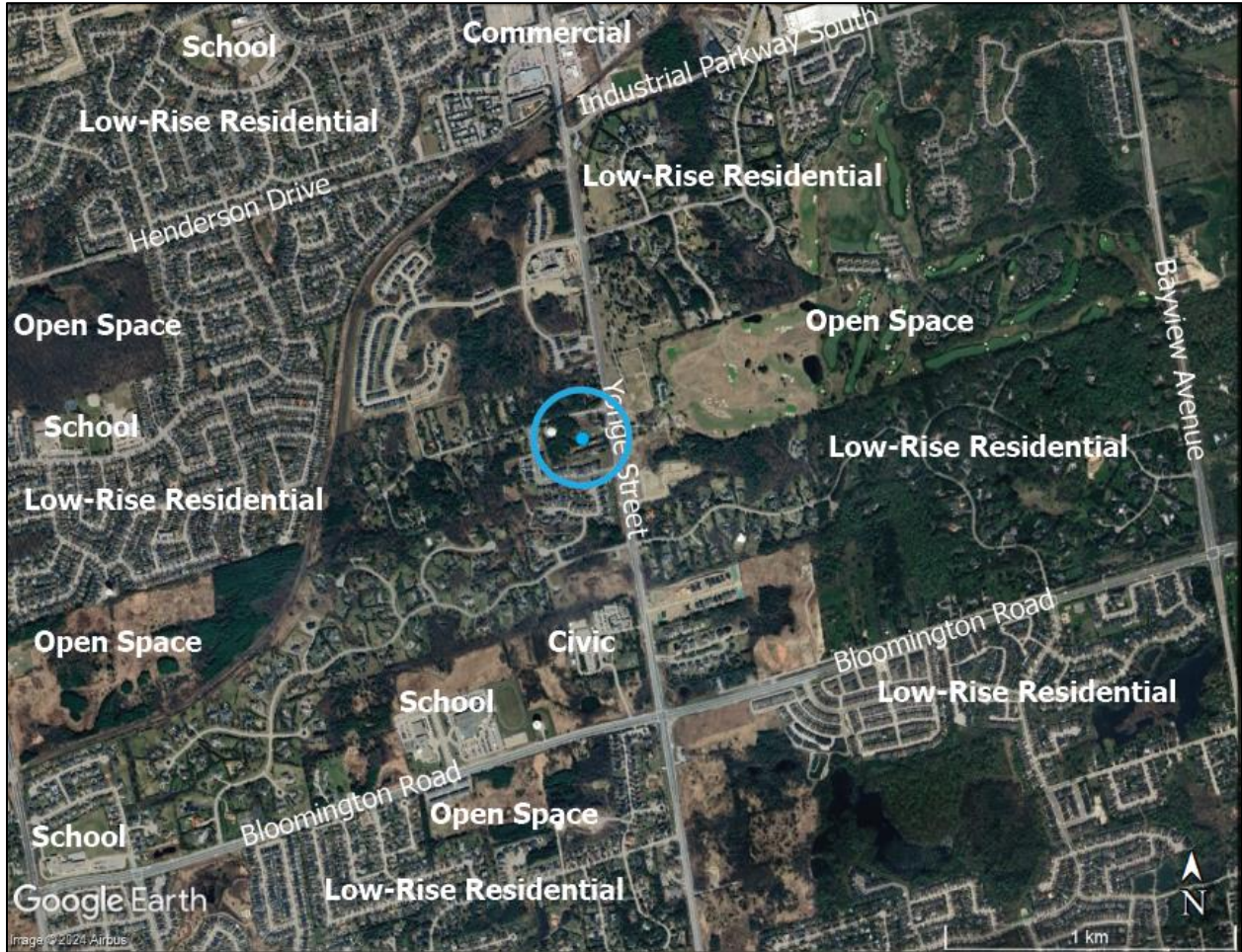
The subject property currently accommodates one (1) single-detached dwelling unit, presently vacant, in addition to a private right-of-way accessing Yonge Street to the east. The existing dwelling will be demolished in conjunction with the proposed development on the lands.

The subject property is generally flat in elevation, with some vegetation including mature and young trees, bushes, and shrubs. This vegetation is concentrated along the subject property's front lot line along Yonge Street, and along its northern site lot line.



Figure 1: Site Aerial

**Figure 2 – Area Land Use Context**  
**14086 Yonge Street, Aurora, ON**



## 3.0 Context Area

### 3.1 Land Use

The subject property is located along a segment of Yonge Street in Aurora which is primarily characterized by low density residential uses. Denser residential uses have been introduced to the area through the development of subdivisions both along Yonge Street and within the surrounding neighbourhoods – indicating a trend of moderate intensification in the area.

#### Immediate Land Uses

**North:** Residential, single detached lot.

**East:** Yonge Street; Beacon Hall Golf Club.

**South:** Single-detached neighbourhood.

**West:** Single-detached neighbourhood.

#### Surrounding Land Uses

**North:** To the north of the subject property are additional low-density residential neighbourhoods, as well as some more recently constructed medium-density developments and the Del Manor Retirement facility along Butternut Ridge Trail. Further north is a commercial centre which accommodates major and minor retail uses, service uses, and institutional uses. Several schools are also located in this area, as well as other open space uses including public parks, and recreational/sport fields.

**East:** The Beacon Hall Golf Course stretches from Yonge Street in the west to Bayview Avenue in the east. The course is surrounded by low density single-detached neighbourhoods with considerable tree coverage.

**South:** The intersection of Yonge Street and Bloomington Road is approximately 1 kilometre south of the subject property. The intersection accommodates newer residential subdivisions, as well as institutional uses including a high school, a Service Ontario office, and Ontario Provincial Police – Aurora station. Open spaces are also concentrated around the intersection, with several parks, trails, and fields in the vicinity.

**West:** The subject property's western context area is characterized by agricultural and rural uses, a CN railroad, and low density single-detached neighbourhoods. Several schools are also located to the west of the subject properties, including primary and secondary, as well as open space uses including parks and golf courses.

The subject property is serviced by various primary and secondary schools in the area, including Highview Public School, Oak Ridges Public School, Ecole Secondaire Catholique Renaissance, Our Lady of the Annunciation Catholic Elementary School, and Cardinal Carter Catholic High School, among others.

### 3.2 Transit Accessibility

The Viva Blue bus route operates along Yonge Street, providing the site with regional and inter-regional transit access along the Yonge Street corridor via the route's stop at Yonge Street and Bloomington Road.

The Viva bus network stop 1 kilometre south of the subject property, representing a walk of approximately 15 minutes. Viva Blue operates from Finch Station in Toronto to the South, to Newmarket Terminal to the north, connecting the subject property to the TTC subway and bus network, as well as to Viva Purple, Pink, and Orange Lines which operate east/west along Highway 7 in Markham and Vaughan. The subject property's access to these transit networks ensures adequate public transit accessibility, and will provide residents with a range of transportation options for their day-to-day lives.

The subject property is additionally serviced by York Region Transit bus routes 96 and 98, which provides public transit access within York Region. Routes 96 and 98 have bus stops approximately 250 metres north of the subject property at the intersection of Yonge Street and Ridge Road – representing a walk of approximately 5 minutes. Route 96 operates from Pioneer Village Station in Vaughan (TTC) north to King City and terminates at Newmarket Terminal. Route 98 operates from Bernard Terminal at Yonge Street and Elgin Mills to the south to Newmarket Terminal to the North.

The variety of public transit routes that service the area ensure that new residential developments will be supported and connected to the many municipalities or York Region, as well as those of neighbouring regions such as Peel, Toronto, Durham. Residents of the proposed development will have a wide variety of options to access their daily needs, recreational activities, and places of employment such that reliance on single-automotive transit is reduced.

### 3.3 Existing Development Applications

The subject property is located in south-central Aurora, where several development applications are currently in process. The class of development applications in the Yonge Street and Bloomington Road area are primarily low-rise residential, including new subdivisions and infill development.

132-198 Old Bloomington Road:	Draft Plan of Subdivision to permit the development of 50 single detached lots. This application has been approved as in under construction.
252, 260, 272 Old Bloomington Road:	Draft Plan of Subdivision, associated OPA, ZBA.
326 Old Bloomington Road:	Official Plan, Zoning By-law Amendment to permit the development of 10 single detached units.
13859-14740 Yonge Street:	Zoning By-law Amendment and Site Plan Application proposing three mixed use apartment buildings of five (5), six (6), and seven (7) storeys to accommodate a total of 876 apartment dwelling units and 2400 square metres of non-residential GFA, as well as 24 three-storey townhouse units. This application has been approved and is in the construction phase.
14029 Yonge Street:	Condominium Application, Draft Plan Extension to permit the development of 27 detached unit condominium.
14288 Yonge Street:	Draft Plan of Subdivision, Zoning By-law Amendment, and Draft Plan of Condominium applications for 11 single detached dwelling units. This application has been approved as of July 9, 2020.

- 14700-14760 Yonge Street: Zoning By-law Amendment and Site Plan Applications to permit the development of 24 townhouse units and a 7-storey apartment building.
- 5-70 Archer Hill Court: Zoning By-law Amendment, and Plan of Subdivision Applications to permit the development of 138 single detached dwellings (Reduced from 145 units, now accommodating revised park space).

The class and quantity of residential development in the area indicates that there is demand for residential densification in the area. Additional residential densification projects have been approved and constructed south of Bloomington Road in Richmond Hill primarily through new subdivisions. Bloomington Road is supported by high public transit accessibility, abundant open space, and proximity to downtown areas with amenities, services, and employment opportunities (Aurora, Oak Ridges in Richmond Hill). These factors render the area as an optimal location for more efficient residential land use.



## 4.0 Description of the Proposal

The proposal seeks to develop twenty (20) three-storey townhomes in three blocks with the following having direct access along the west side of Yonge Street. As depicted in the submitted Site Plan, the required parking spaces will be facilitated within an underground parking level that will provide direct access to each of the individual units.

The proposal will also provide for a total of 15 surface parking spaces that will be aligned in a parallel fashion along the south limits of the internal laneway.

Another important design element of this proposal (as depicted in the submitted Conceptual Site Plan) is its ability to facilitate the future development of the abutting lands to the north, via the shared Yonge Street access and the extension of the internal laneway.

The development's proposed statistics are as follows:

Standard	Proposal
<b>Lot Area</b>	3,951.45m <sup>2</sup>
<b>Lot Frontage</b>	25.23m
<b>Front Yard Setback-To Yonge Street</b>	14.90m
<b>Rear Setback</b>	6.50m
<b>Building Depth</b>	7.91m
<b>Building Height</b>	12.0m
<b>Storeys</b>	3
<b>Total GFA</b>	200.83m <sup>2</sup>
<b>Building Coverage</b>	25.76%
<b>Paved Area Coverage</b>	38.12%
<b>Landscaped Area</b>	28.92%
<b>Units</b>	20
<b>Parking Spaces</b>	40 + 15 visitor parking spaces

Table 1: Proposed Building & Lot Standards

The development proposal on the subject property of 14086 Yonge Street is appropriate in that it:

- ❖ Provides infill development in an area with high regional and inter-regional public transit accessibility;
- ❖ Contributes towards the Region of York and Town of Aurora's growth targets;
- ❖ Is located along Yonge Street – a higher order transit corridor with multiple bus routes and connectivity to surrounding municipalities;
- ❖ Provides infill development on an underutilized lot;
- ❖ It conforms to the policies of the Provincial Policy Statement, Growth Plan and the Region of York Official Plan;
- ❖ The proposed Medium Density Residential use complies with and reinforces the intent of the Official Plan policy to create developments in a compact and orderly form;
- ❖ It will be a catalyst for redevelopment in this segment of the town, as it will facilitate the redevelopment and optimal land use of an underutilized parcel of land with direct access to the Yonge Street Regional Corridor;

- ❖ It provides for appropriate infill development in an area with significant infrastructure and transit and it will provide the Town of Aurora with much needed broader range and mix of housing in an appropriate location;
- ❖ The proposed site design and lot fabric shall not preclude future development on the northern-abutting lot, where the provision of a private right-of-way along the subject property's northern side lot line shall facilitate vehicular access to said abutting property and any potential residential homes constructed upon it;
- ❖ The proposal will prominently provide for family oriented three (3) bedroom units;
- ❖ As the supporting documentation indicates, there are no unacceptable adverse impacts, particularly from a traffic, municipal servicing, environmental and land use compatibility perspectives, to the surrounding land uses, given the property's prominent location on the Yonge Street corridor;
- ❖ Provides for a sufficient number of on-site vehicular parking spaces; and,
- ❖ It provides sufficient building setbacks from both a buffering and streetscape perspective.

To implement this proposal, a Zoning By-law Amendment Application is required that will address site specific regulations such as setbacks and building height.

One technique available and recommended for the Zoning By-law Amendment is to amend By-law No. 6000-17 to rezone the subject lands from "Rural – Oak Ridges Moraine (RU-ORM) Zone" to "Townhouse Dwelling Residential (R8) Zone" with the applicable site-specific development standards.

## 5.0 Public Consultation Strategy

To adequately inform the public and to keep them involved in the planning process, in addition to the mandatory Public Hearing that will be held as outlined in the Planning Act, a Public Open House/Neighbourhood Consultation Meeting is recommended. This will allow for a more informal opportunity for any interested parties to come out, view and comment on the application. This event can be scheduled by either the applicant and/or with the assistance from the Local Councillor.

The applicant has the following objectives they wish to accomplish by engaging and consulting with the public regarding their proposal.

- ❖ Share information related to the proposal with the public and any interested stakeholders;
- ❖ Provide all interested persons and groups, the opportunity to participate in the engagement process and provide feedback by using various methods of engagement;
- ❖ Identify common themes and patterns from the participants and work to incorporate this feedback into the further refinement of the development scheme;
- ❖ Communicate with the public in a transparent and open manner about the proposal as well as the engagement process;
- ❖ Work in a collaborative manner with all stakeholders and interested parties throughout the engagement process; and,
- ❖ Report back to the public with the outcome of the consultation strategy.

## 6.0 Policy Regime

### 6.1 Provincial Policy Statement 2020

On February 28, 2020, the Ministry of Municipal Affairs and Housing released the Provincial Policy Statement 2020 which came into effect May 1, 2020. The Provincial Policy Statement (PPS) provides policy direction on matters of provincial interest related to land use planning and development. The PPS aims to permit appropriate development while ensuring that resources of provincial interest, public health and safety, and the quality of the natural environment are protected. All planning decisions in Ontario must be "consistent" with the policies of the PPS.

The PPS encourages efficient land use planning and growth management to create and maintain strong communities and a healthy environment while encouraging economic growth over the long term. The PPS also encourages the efficient use of existing infrastructure and public service facilities and requires that municipalities plan for an appropriate range and mix of land uses throughout the Province.

Section 1.0 includes policy direction related to *"Building Strong Communities"* and is applicable to the subject property. It generally encourages a variety of land uses within communities and encourages initiatives that make efficient use of infrastructure and support the local community.

Section 1.1 of the PPS promotes the management of land uses in an effort to achieve efficient development and overall land use patterns, specifically that health; livable and safe communities are sustained by:

- "1.1.1 a) *promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) *accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*

- g) ensuring that necessary infrastructure, and public service facilities are or will be available to meet current and projected needs;*
- h) promoting development and land use patterns that conserve biodiversity; and,*
- i) preparing for the regional and local impacts of a changing climate.”*

Furthermore, Section 1.1.3 of the PPS discusses the several policies surrounding settlement areas and appropriate development within them specifically:

- "Section 1.1.3.1 Settlement areas shall be the focus of growth and development.*
- "Section 1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:*
- a) efficiently use land and resources;*
  - b) are appropriate for and efficiently use, the infrastructure and public service facilities which are planned or available and avoid the need for their unjustified and/or uneconomical expansion;*
  - c) minimize negative impacts to air quality and climate change and promote energy efficiency;*
  - d) prepare for the impacts of a changing climate;*
  - e) support active transportation;*
  - f) are transit-supportive, where transit is planned, exists or may be developed; and,*
  - g) are freight-supportive.*
- "Section 1.1.3.4 Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.*
- "Section 1.1.3.6 New Development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.*
- "Section 1.1.3.6 New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities."*

Section 1.4.3 of the PPS provides guidance on the provision of housing, where:

- "Section 1.4.3 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:*

- c) *directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*
- d) *promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;*
- e) *requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations."*

Section 1.6 of the PPS discusses the efficient use of existing and future infrastructure and public service facilities. This Section contains the following key policies:

*"Section 1.6.1           Infrastructure and public service facilities shall be provided in an efficient and manner that prepares for the impacts of a changing climate while accommodating project needs.*

*Planning for infrastructure and public service facilities shall be coordinated and integrated with land use planning and growth management so that they are:*

- a) *financially viable over their life cycle, which may be demonstrated through asset management planning; and,*
- b) *available to meet current and projected needs."*

*"Section 1.6.2           Planning authorities should promote green infrastructure to complement infrastructure. "*

*"Section 1.6.3           Before consideration is given to development new infrastructure and public service facilities:*

- a) *The use of existing infrastructure and public service facilities should be optimized; and,*
- b) *opportunities for adaptive re-use should be considered, wherever feasible.*

The PPS in Section 1.6.7 discusses the importance of transportation systems and identifies the need for providing safe and efficient means of moving people and good. The key policies in the section are as follows:

*"Section 1.6.7.2       Efficient use should be made of existing and planned infrastructure including through the use of transportation demand management strategies, where feasible;*

*"Section 1.6.7.4 A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation; and,*

An additional goal of the PPS is to promote energy conservation and air quality control as a response to climate control. These policies are discussed in Section 1.8 of the PPS:

*"Section 1.8.1 Planning authorities shall support energy conservation and efficient, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:*

- a) promote compact form and a structure of nodes and corridors;*
- b) promote the use of active transportation and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas;*
- e) encourage transit-supportive development and intensification to improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion."*

The proposed development conforms with the policies of the PPS in that it:

- ❖ The proposal will make full and cost-effective use of the existing infrastructure, public transit, and human services systems within a highly urbanized environment. Additionally, the proposed development is located within close proximity to existing urban amenities and public transportation facilities thereby maximizing opportunities for non-automobile modes of travel by generating alternative means of transportation (i.e., walking, cycling, or public transit);
- ❖ The proposed development can more readily provide an opportunity to accommodate and contribute to a broader range and mix of housing units, therefore meeting the needs of the municipality's varied population from a social, cultural, and economic perspective without any land use patterns that would cause any adverse environmental or public health and safety issues;
- ❖ Contemplates efficient infill development that contributes to an appropriate increase in density that would not cause any environmental or public health and safety issues;
- ❖ Provides housing type diversity through townhouse dwellings, where the area's neighbourhoods are primarily characterized by single-detached lots;
- ❖ Provides for development on an existing underutilized residential lot which will efficiently use the land, resources, infrastructure and public service facilities;
- ❖ Provides for an appropriate development that is not within a Natural Hazard Area;
- ❖ It is in close proximity of existing urban amenities and public transportation facilities thereby maximizing opportunities for alternative modes of active transportation; and,
- ❖ Provides for a conversion of an underutilized lot which will benefit the local area and will assist in creating a more compact and complete community.

The subject application contributes to the provision of an appropriate range and mix of residential housing, to meet long term needs of this area.

In summary, the subject applications are supportive of, consistent with, and advance the policy initiatives of the PPS.

## 6.2 Growth Plan for the Greater Golden Horseshoe 2020

A Place to Grow: Growth Plan for the Greater Golden Horseshoe (the “Growth Plan”) came into effect on May 16, 2019. It replaced the Growth Plan, 2017, and was prepared under the Places to Grow Act, 2005. Amendment 1 (2020) to the Growth Plan for the Greater Golden Horseshoe, 2019 came into effect on August 28, 2020. The Government of Ontario implemented the new Growth Plan for the Greater Golden Horseshoe (Growth Plan) which serves as the culmination of a coordinated review by the province, taking into consideration feedback from stakeholders, the public and indigenous communities.

The updated Growth Plan provides the framework to implement the Provincial vision for the establishment of strong, prosperous communities to the year 2051. The Greater Golden Horseshoe has been identified as one of North America’s fastest growing regions and it is important that growth be managed and directed in an appropriate manner.

This updated version of the original 2006 Growth Plan and updated 2017 version combines to remove, revise, and add brand new policies to better suit and reflect the province’s goals and objectives for land use planning in relation to the updated time horizon to 2041. As such, all decisions made on or after August 28, 2020, shall be in conformity with this Plan.

The updated document’s primary growth-related objectives are identified in Section 2.1 – Context, in which includes among others, the following:

*“This Plan is about accommodating forecasted growth in complete communities. These are communities that are well designed to meet people’s needs for daily living throughout an entire lifetime by providing convenient access to an appropriate mix of jobs, local services, public service facilities, and a full range of housing to accommodate a range of incomes and household sizes.”*

*“Better use of land and infrastructure can be made by directing growth to settlements areas and prioritizing intensification, with a focus on strategic growth areas, including urban growth centres and major transit station areas, as well as brownfields and greyfields. Concentrating new development in these areas provides a focus for investments in transit as well as other types of infrastructure and public service facilities to support forecasted growth, while supporting a more diverse range and mix of housing options”.*

As will be further discussed in Section 6.6 of this report, the subject site is located within the “Village Core” and “Transit Station” areas under the Township of King’s Official Plan, establishing it within an area targeted to receive greater densification – similar to the Growth Plan’s objectives for Delineated Built-Up Areas. The subject property is further in an area that is supported by existing transit infrastructure, establishing it as a target for development and intensification.

Section 2.2.1 of the Growth Plan, which discusses the management of growth in regards to where and how development should occur within the GGH, has implemented several changes as follows:

1. *“Population and employment growth forecasts contained in Schedule 3 will be used for planning and managing growth in the GGH to the horizon of this Plan in accordance with the policies in subsection 5.2.4.*
2. *Forecasted growth to the horizon of this Plan will be allocated based on the following:*
  - a. *The vast majority of growth will be directed to settlement areas that:*
    - i. *Have a delineated built boundary;*
    - ii. *Have existing or planned municipal water and wastewater systems; and*
    - iii. *Can support the achievement of complete communities;*
  - b. *Growth will be limited in settlement areas that;*
    - i. *Are rural settlements;*
    - ii. *Are not served by existing or planned municipal water and wastewater systems;*  
*or*
    - iii. *Are in the Greenbelt Area;*
  - c. *Within settlement areas, growth will be focused in:*
    - i. *Delineated built-up areas;*
    - ii. *Strategic growth areas;*
    - iii. *Locations with existing or planned transit, with a priority on higher order transit where it exists or is planned; and*
    - iv. *Areas with existing or planned public service facilities;*
  - d. *Development will be directed to settlement areas, except where the policies of this Plan permit otherwise;*
  - e. *Development will be generally directed away from hazardous lands; and*
  - f. *The establishment of new settlement areas is prohibited.*
3. *Upper and single-tier municipalities will undertake integrated planning to manage forecasted growth to the horizon of this Plan, which will:*
  - a. *Establish a hierarchy of settlement areas, and of area within settlement areas, in accordance with policy 2.2.1.2;*
  - b. *Be supported by planning for infrastructure and public service facilities by considering the full life cycle costs of these assets and developing options to pay for these costs over the long-term;*
  - c. *Provide direction for an urban form that will optimize infrastructure, particularly along transit and transportation corridors, to support the achievement of complete communities through a more compact built form;*
  - d. *Environmental and agricultural protection conservation objectives of this Plan and*
  - e. *Be implemented through a municipal comprehensive review and, where applicable, include direction to lower-tier municipalities.*
4. *Applying the policies of this Plan will support the achievement of complete communities that:*
  - a. *Feature a diverse mix of land uses, including residential and employment uses, and convenient access to local stores, services and public facilities;*

- b. *Improve social equity and overall quality of life, including human health, for people of all ages, abilities and incomes;*
- c. *provide a diverse range and mix of housing options, including additional residential units and affordable housing, to accommodate people at all stages of life, and to accommodate the needs of all household sizes and incomes;*
- d. *Expand convenient access to:*
  - i. *A range of transportation options, including options for the safe, comfortable, and convenient use of active transportation;*
  - ii. *Public service facilities, co-located and integrated in community hubs;*
  - iii. *An appropriate supply of safe, publicly accessible open spaces, parks, trails, and other recreational facilities; and*
  - iv. *Healthy, local, and affordable food options, including through urban agriculture.*
- e. *Provide for a more compact built form and a vibrant public realm, including public open spaces;*
- f. *Mitigate and adapt to climate change impacts, improve resilience, and reduce greenhouse gas emissions, and contribute to environmental sustainability; and,*
- g. *Integrate green infrastructure and low impact development”.*

Section 2.2.2 discusses the objective of intensification within Delineated Built-Up Areas which includes the following:

1. *“By the time the next municipal comprehensive review is approved and in effect, and for each year thereafter, the applicable minimum intensification target is as follows:*
  - a. *A minimum of 50 per cent of all residential development occurring annually within each of the Cities of Barrie, Brantford, Guelph, Hamilton, Orillia and Peterborough and the Regions of Durham, Halton, Niagara, Peel, Waterloo, and York will be within the delineated built-up area; and*
2. *Until the next municipal comprehensive review is approved and in effect, the annual minimum intensification target contained in the applicable upper- or single-tier official plan that is approved and in effect as of July 1, 2017, will continue to apply.*
3. *All municipalities will develop a strategy to achieve the minimum intensification target and intensification throughout delineated built-up areas, which will:*
  - b. *Identify the appropriate type and scale of development in strategic growth areas and transition of built form to adjacent areas;*
  - d. *Ensure lands are zoned and development is designed in a manner that supports the achievement of complete communities;*
  - e. *Prioritize planning and investment in infrastructure and public service facilities that will support intensification; and,*
  - f. *Be implemented through official plan policies and designations, updated zoning, and other supporting documents.*

Section 2.2.5.5: Municipalities should designate and preserve lands within settlement areas located adjacent to or near major goods movement facilities and corridors, including major highway interchanges, as areas for manufacturing, warehousing and logistics, and appropriate associated uses and ancillary facilities.

The proposed development conforms to the policies of the Growth Plan in that it:

- ❖ Is located within a “Built-Up Area” which is designed to promote development;
- ❖ The development is intensifying underutilized land in a redeveloping area;
- ❖ The proposed development on the lands will make full and cost-effective use of the existing local and provincial infrastructure, public transit, and human service systems. The buildings are well designed, efficiently located, and highly accessible and will provide people with alternative means of transportation (i.e., walking, cycling, or public transit) thus reducing local automobile trips, increasing the modal split, and improving the overall streetscape along this portion of Yonge Street;
- ❖ The proposal aims to provide appropriate intensification in an infill lot;
- ❖ Assists with the Regional Municipality of York, and the Town of Aurora’s intensification targets;
- ❖ Makes use of existing municipal water and wastewater systems as well as other forms of municipal infrastructure;
- ❖ Provides for intensification along existing bus lines along Yonge Street between the Viva Transit Bernard and Newmarket Terminals; and,
- ❖ Contributes towards the Growth Plan’s target of 50% of new residential development taking place in the delineated built-up-area of the City.

The Growth Plan also considers public infrastructure and the sufficiency of infrastructure services. The proposed development is consistent with the intent of intensification within urbanized areas that have sufficient public infrastructure to support new development. The proposed development conforms to the Growth Plan and represents an appropriate intensification of residential uses that is compatible with surrounding land uses. The proposed development effectively implements an urban, complete, and compact community.

In summary, the subject application is supportive of and conforms to the policies of the Growth Plan.

### 6.3 Greenbelt Plan

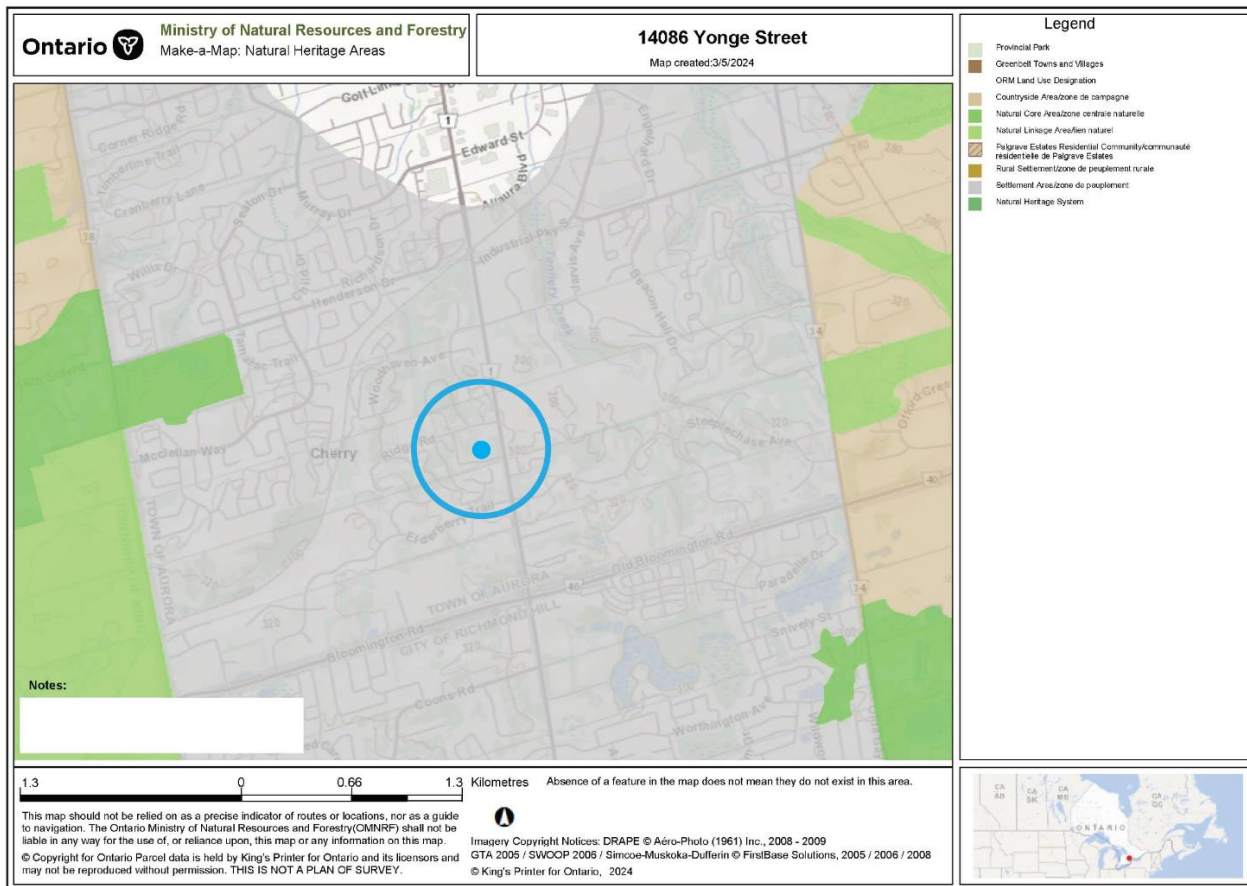
The most recent version of the Greenbelt Plan was approved by the Lieutenant Governor in Council, Order in Council No. 1025/2017 as of July 1, 2017 under the *Greenbelt Act, 2005*. The objective of the Plan is to protect against the loss and fragmentation of the agricultural land base and supports agriculture as the predominant land use, to protect natural heritage and water resource systems, to provide a diverse range of economic and social activities associated with rural communities, agriculture, tourism, recreation and resource uses, as well as to built resilience and to mitigate climate change.

The subject property is located within a Settlement Area, which are designated to accommodate social, economic and service functions through residential, institutional and commercial/industrial use needed by current, and future population within the Greenbelt. Settlement Areas are further mandated to

support complete communities which are in transit supportive community hubs that are economically and environmentally sustainable with the goal of low carbon-built form.

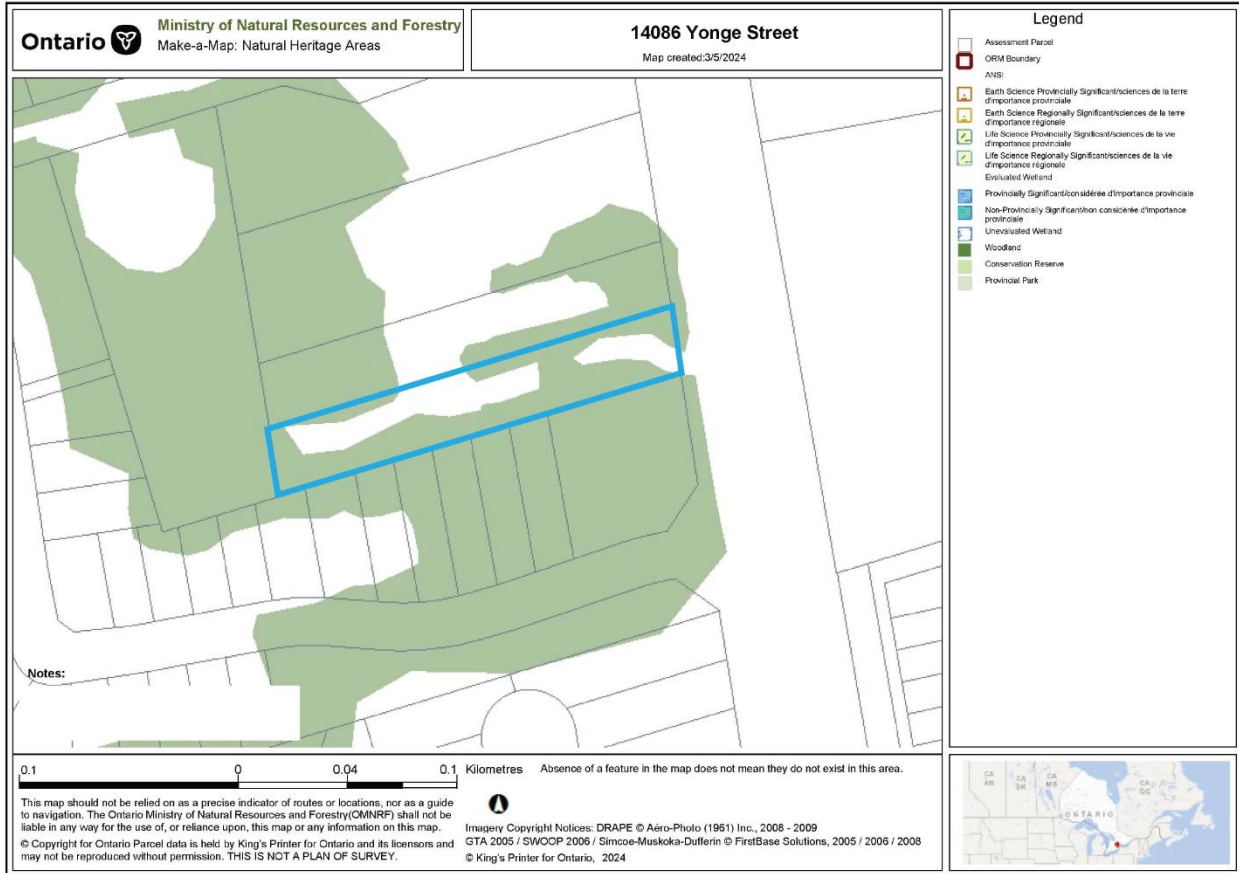
The subject property is located within the Oak Ridges Moraine Area as per Greenbelt Plan Schedule 1. Section 2.1 of the Plan addresses Lands within the Oak Ridges Moraine Area made under the *Oak Ridges Moraine Conservation Act, 2001* (ORMCP). The subject property is located within the ORM Area and applicable policies will be discussed in Section 6.4 of this report.

**Figure 4 – ORM Land Use**  
**14086 Yonge Street, Aurora, ON**



Source: Oak Ridges Moraine Conservation Plan Land Uses

**Figure 5 – Natural Heritage Map  
14086 Yonge Street, Aurora, ON**



## 6.4 Oak Ridges Moraine Conservation Plan

The Oak Ridges Moraine Conservation Plan was approved in 2017 and is set out under the Oak Ridges Moraine Conservation Act, 2001. The Conservation Plan includes a reproduction of the Conservation Act and Introduction and Implementation sections, which provide both an explanation of the Plan and additional information for its users. The purpose of the Plan is to provide land use and resource management planning direction on how to protect the Moraine’s ecological and hydrological features and functions.

The Oak Ridges Moraine is a significant landform that shapes the present and future form and structure of the Greater Toronto region, while its ecological functions are critical to the region’s continuing health – with a concentration of environmental, geological, and hydrological features that make its ecosystem vital to south-central Ontario.

The Plan’s objectives are to protect, maintain, and improve the Moraine’s ecological and hydrological integrity through optimal land usage, maintaining existing open spaces and recreational trails, and through ensuring that only land and resource uses that contribute to the ecological and hydrological functions of the Moraine are permitted.

The subject property falls under the “Settlement Area” land use designation, which is to “*reflect a range of existing communities planned by municipalities to reflect community needs and values. Urban use and development as set out in municipal official plans are allowed.*” Policy guidelines for Settlement Areas are outlined under Section 18 of the Act:

1. *“The purpose of Settlement Areas is to focus and contain urban growth by,*
  - a. *encouraging the development of communities that provide their residents with convenient access to an appropriate mix of employment, transportation options and local services and a full range of housing and public service facilities, and minimizing the encroachment and impact of development on the ecological functions and hydrological features of the Plan Area;*
  - b. *promoting the efficient use of land with transit-supportive densities, through intensification and redevelopment within existing urban areas; and*
  - c. *providing for the continuation and development of urban land uses consistent with the growth management strategies identified in the applicable official plans.*
2. *Settlement Areas also have the objectives of,*
  - a. *maintaining, and where possible improving or restoring, the health, diversity, size and connectivity of key natural heritage features, key hydrologic features and the related ecological functions;*
  - b. *accommodating a trail system through the Plan Area and trail connections to it;*
  - c. *promoting strong communities, a strong economy and a healthy environment;*
    - i. *promoting the locating of two or more compatible public services in one building or place that is conveniently situated so as to be accessible to local residents by walking, cycling and, where available, public transit;*
    - ii. *ensuring that development takes place in a manner that reduced greenhouse gas emissions;*

- iii. *conserving cultural heritage resources;*
- iv. *ensuring the sustainable use of water resources; and providing for economic development that is compatible with subsection (1) and clauses (a) to (c.4)."*

The Plan further establishes minimum areas of influence and minimum vegetation protection zones for areas abutting, or within a certain distance of key natural heritage features and key hydrologic features. The subject property does not contain any KNHFs or KHFs, making the setback requirements under this Plan inapplicable.

Under the Conservation Plan, the subject property is located within a Category 1 "Complex" Landform Conservation Area. Section 30 for the plan, where the following applies:

*"Section 30.5: An application for development or site alteration with respect to land in a landform conservation area (Category 1) shall identify planning, design and construction practices that will keep disturbance to landform character to a minimum, including,*

- a) Maintaining significant landform features such as steep slopes, kames, kettles, ravines and ridges in their natural undisturbed form;*
- b) Limiting the portion of the net developable area of the site that has impervious surfaces to not more than 15 percent of the total area of the site."*

*Section 30.8: An application for major development with respect to land in a landform conservation area of either category shall be accompanied by a landform conservation plan that shows, on one or more map,*

- a) elevation contours in sufficient detail to show the basic topographic character of the site, with an interval of not more than two metres*
- b) analysis of the site by slope type (for example, moderate or steep);*
- c) significant landform features such as kames, kettles, ravines and ridges; and*
- d) all water bodies including intermittent streams and ponds."*

While the above policies of Section 30.8 apply, Section 30.13 provides further regulation for development on Settlement Areas within landform conservation areas, where:

*"Section 30.13: With respect to land in Settlement Areas, in considering applications for development or site alteration within landform conservation areas (Category 1 and 2) the approval authority shall consider the importance of adopting planning, design and construction practices that will keep disturbance to landform character to a minimum, so as to satisfy the requirements of subsections (5) to (11) if possible."*

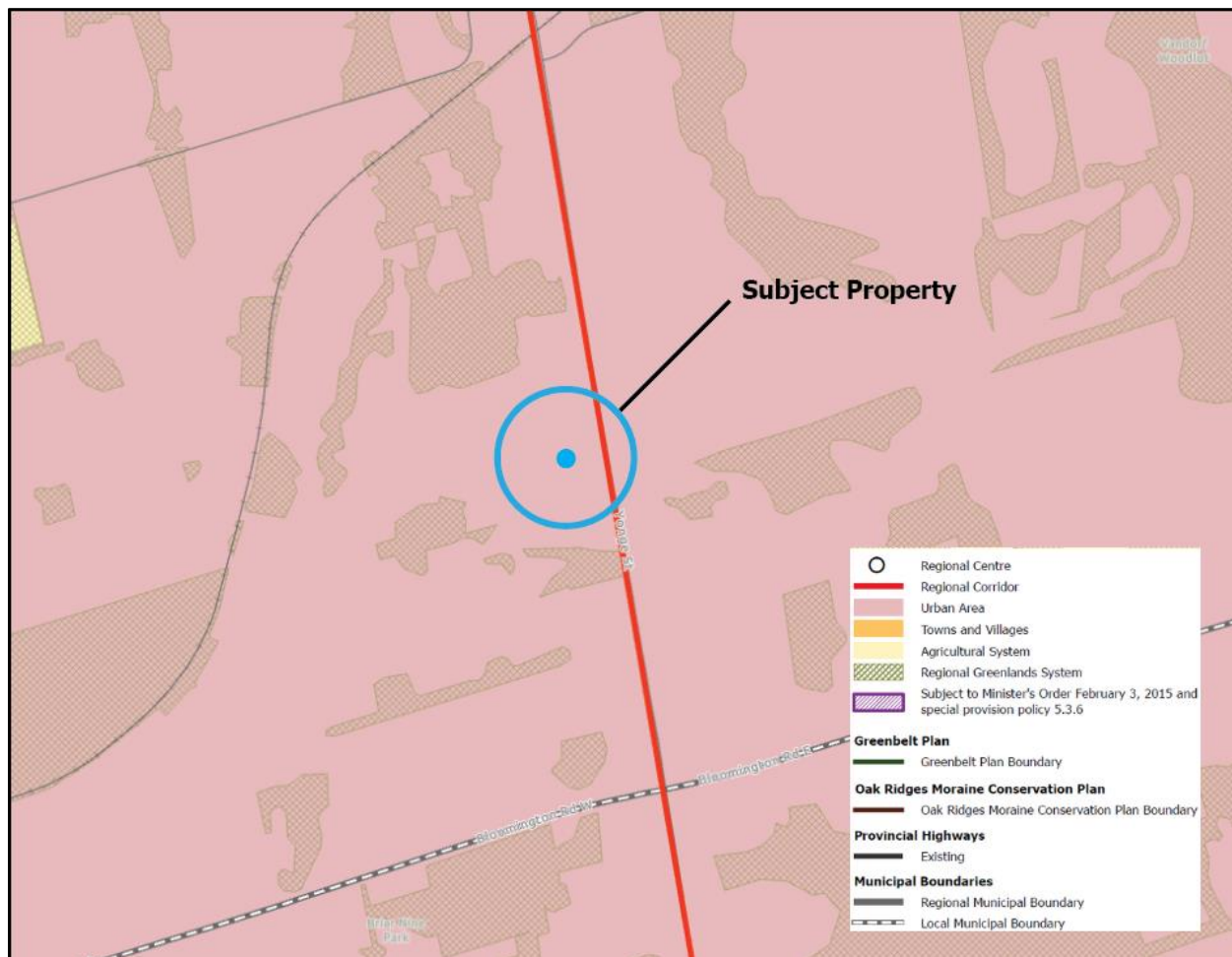
Section 30.13 suggests that lands within Settlement Areas that are also within a Category 1 or 2 Landform shall be sensitive to the unique requirements associated with said Landforms, but that planning bodies must simultaneously acknowledge the land's context within an urban area and their potential to accommodate more efficient land use that optimizes existing utilities and services.

The subject property does not contain, nor abut, wetlands, significant woodlands, permanent or intermittent streams, or other such KNHFs or KHF, as confirmed by the Environmental Impact Study (EIS) and Natural Heritage Evaluation (NHE) that have been included in this submission.

The proposed development will effectively implement the above-noted Oak Ridges Morain Conservation Plan policies in the following manner:

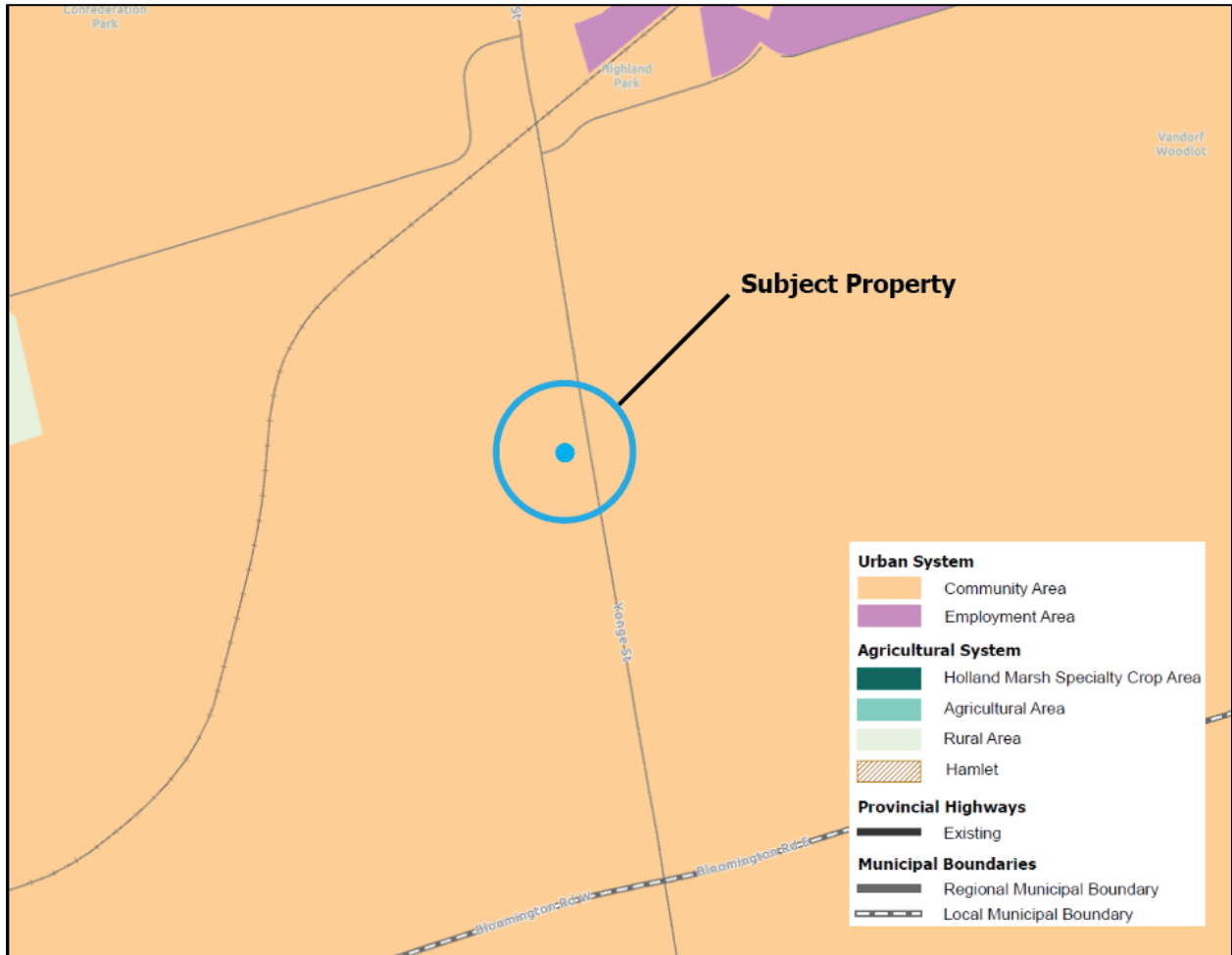
- ❖ Provides moderate residential development within a Settlement Area;
- ❖ Provides efficient use of land which is supported by local and regional transit networks;
- ❖ Is consistent with local and regional growth management strategies; and,
- ❖ Does not disrupt natural heritage features or recourses.

**Figure 6 – YROP Regional Structure**  
**14086 Yonge Street, Aurora, ON**



Source: York Region Official Plan Map 1 – Regional Structure

**Figure 7 – YROP Land Use**  
**14086 Yonge Street, Aurora, ON**



Source: York Region Official Plan Map 2 – Land Use

## 6.5 York Region Official Plan 2022

The 2022 York Region Official Plan (YROP) has been approved as of June 10, 2022, with modifications by the Province of Ontario's Ministry of Municipal Affairs and Housing. With the update of the YROP, residents and stakeholders were invited to participate in sharing the Region's future communities through public engagement activities. The proposed YROP was adopted by Regional Council on June 30, 2022.

The YROP designates the subject property as "Urban Area" in accordance with Map 1 – Regional Structure. The "Urban Area" designation aims to provide stable residential, and employment uses that assist in meeting the growth forecast of the Region while also providing for a unique sense of identity. The subject property is further designated as a "Community Area" in accordance with Map 1A. "Community Areas" are described as areas where the majority of residents, personal services, retail, arts, culture, recreational facilities and human service needs will be located, as well as employment opportunities.

Section 3.5 of the YROP discusses the importance of housing residents and makes a key point in that *"the housing market is faced with demand for a broader variety of housing types and sizes to meet the diverse range of needs of the Region's residents and workers. Meeting these needs allows people to live and work in their communities as they progress through the stages of life"*. It also adds that *"local municipal official plans and zoning by-laws [should] permit a mix and range of housing types, lot sizes, unit sizes, functions, tenures and levels of affordability within each community"*.

Section 4.2 of the YROP discusses community areas, stating that they are to *"achieve a balance of residential and employment opportunities to provide opportunities for living, working, and daily activities in close proximity to one another"* – enhancing the quality of life for residents and visitors to York Region. Emphasis is placed on creating transit-supported development that encourages active transportation, and which incorporates elements that promote a sense of place within the community.

As per Map 1 of the YROP, the subject property is located along the "Regional Corridor" of Yonge Street. "Regional Corridors" are described under Section 4.4 of the YROP as *"the primary location for connecting Regional transportation corridors to enhance the mobility of people and goods, to, from, and within York Region."* They are further described as *"diverse places serviced by rapid transit that support a range and mix of activities that enrich the character and meet the needs of the communities located along the Regional Corridors."* The YROP acknowledges that the character and pace of development along Regional Corridors will differ based on their context, while playing an important role in the provision of intensification as "Strategic Growth Areas" – specifying that *"development applications along Regional Corridors outside of Major Transit Station Areas shall have regard to local context and impact on achieving regional intensification in the hierarchy of Section 4.1.3,"* which places Regional Corridors outside of MTSAAs as fourth (out of five) on said hierarchy.

Section 4.4 of the YROP addresses intensification, which is defined in this Plan as *"the development of a property, site, or area at a higher density than currently exists through... the development of vacant and/or underutilized lots within previously developed areas."* The subject property's location along a Regional Corridor and rapid transit corridor establishes it as a target for intensification as a strategic growth area, which requires strong access to employment and services under section 4.4.7.

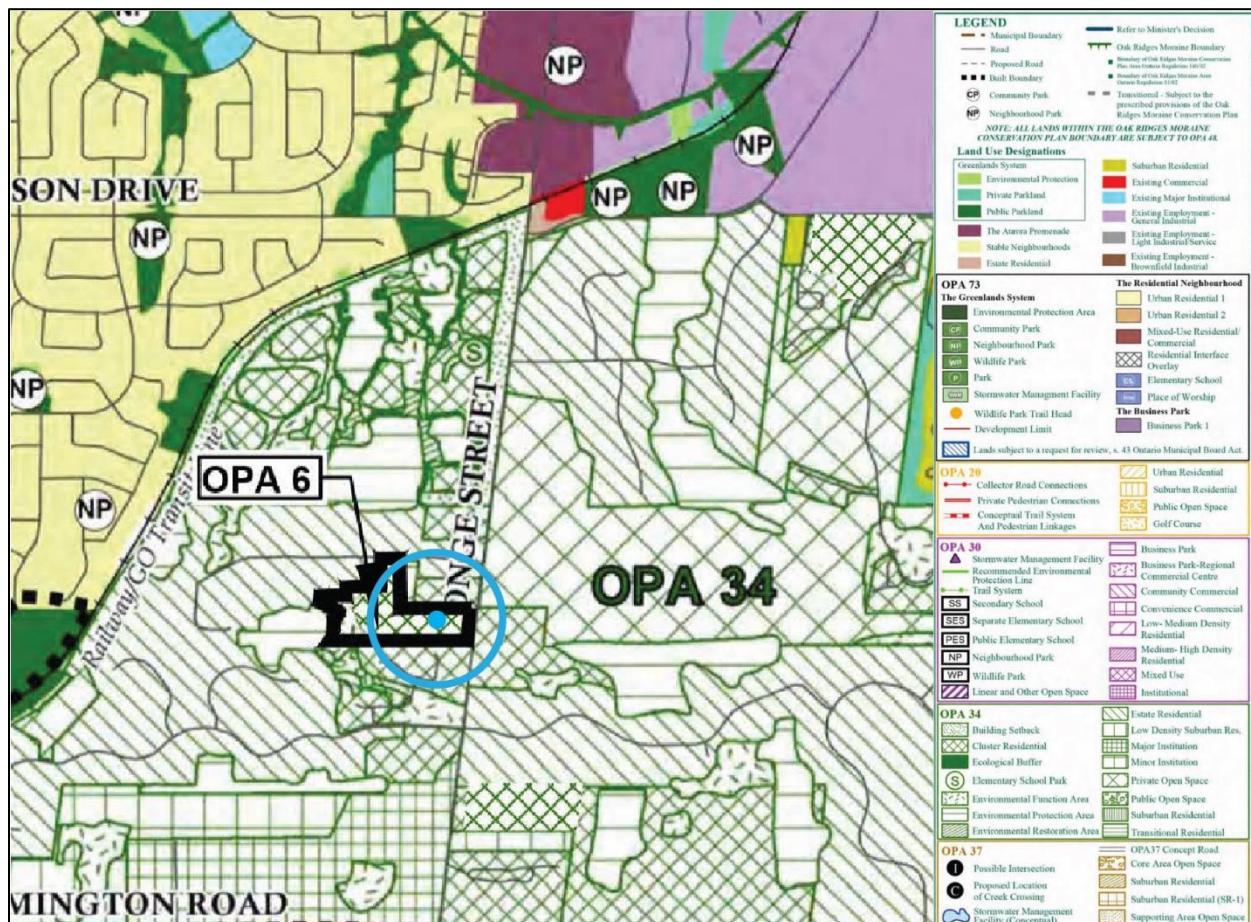
Table 6 under Section 4.4.10 of the YROP further assigns a 45% intensification rate (4,600 units) to Aurora planned to be reached through gentle density along major streets and through the provision of a range and mix of housing that is commensurate with the planned density and local context.

The subject property's location along the Yonge Street Rapid Transit Corridor furthermore grants it access to multi-modal transit facilities, as per Section 2.1 of this report. Areas with access to multi-modal transit are planned to support higher density development under Section 4.4.14.

The proposed development will effectively implement the above-noted Regional policies in the following manner:

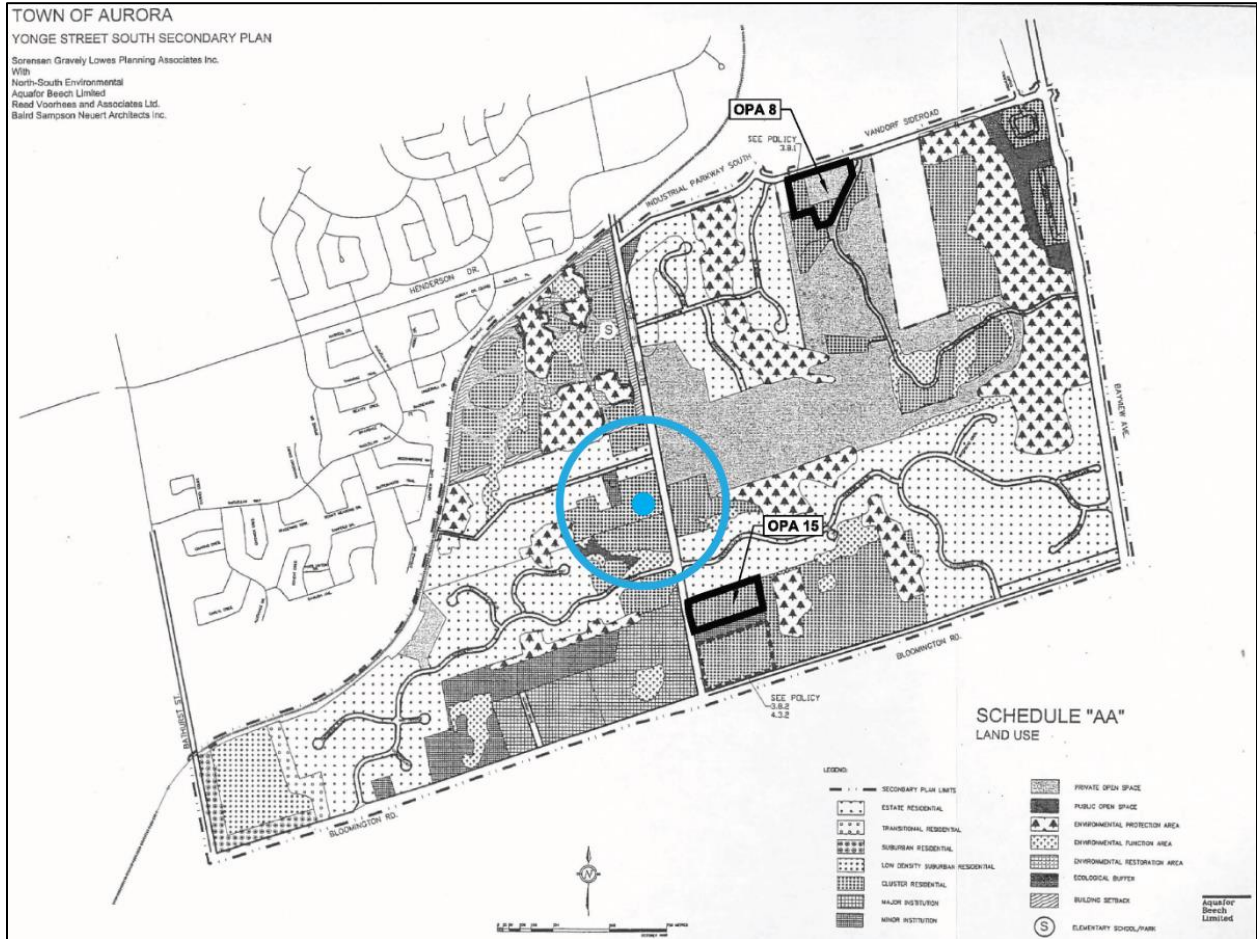
- ❖ Provides residential intensification within an "Urban Area" designation – which is designated to promote efficient land use and residential densification;
- ❖ Assists in reaching the Region's growth target by supporting the creation of additional residential dwelling units;
- ❖ Provides transit supportive intensification along a Regional Corridor (Yonge Street), with access to the VIVA Blue Line (York Region Transit) and within 250 metres of a bus stop – that provides local public transit access and connectivity to regional transit nodes;
- ❖ Provides development on an underutilized property;
- ❖ By supporting a mix and range of unit and lot sizes;
- ❖ By providing a modest increase in residential density which will support local economic activities and public transit in the area;
- ❖ By providing development that is compatible with surrounding land uses, and built form, as well as consistent with the character of current development applications;
- ❖ By providing gentle infill development within an area with access to a variety of services, commercial and retail opportunities, recreational, and institutional uses, and which is located in close proximity to a Regional Corridor;
- ❖ By assisting the Town to achieve the target of having 90% of all residents live within 500m of a public transit stop
- ❖ By assisting with the achievement of the Town of Aurora's growth targets for 2031 and beyond; and,
- ❖ By facilitating the redevelopment of an underutilized parcel of land for optimal use.

**Figure 8 – Aurora OP Land Use  
14086 Yonge Street, Aurora, ON**



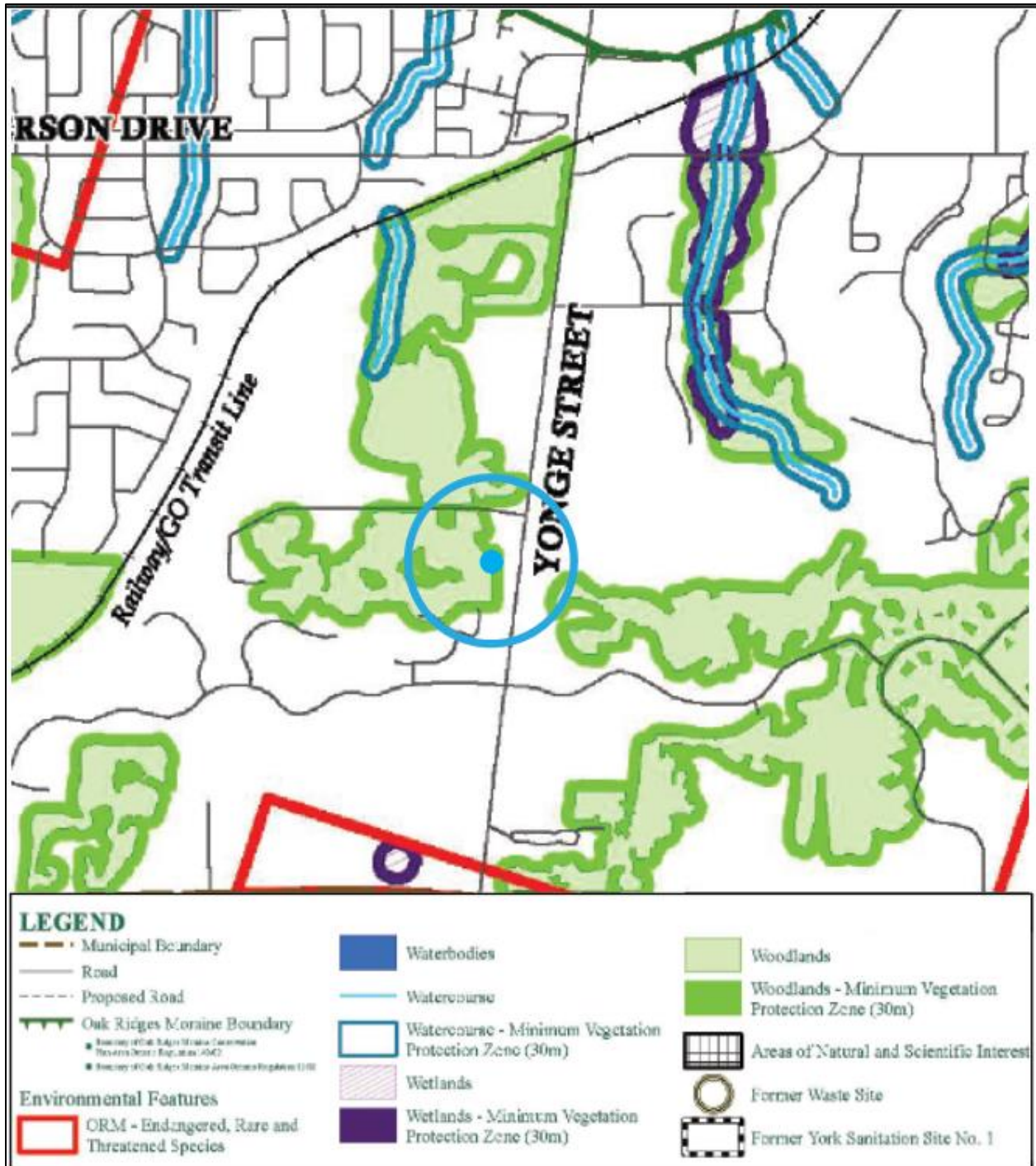
Source: Town of Aurora Official Plan Schedule 'A'

**Figure 9 – Aurora OPA 34 Land Use**  
**14086 Yonge Street, Aurora, ON**



Source: Town of Aurora Official Plan Amendment No. 34 Land Use Map

Figure 10 – Aurora OP Environmental Designations  
14086 Yonge Street, Aurora, ON



Source: Town of Aurora Official Plan Schedule 'E1'

## 6.6 Town of Aurora Official Plan 2010

The Town of Aurora Official Plan has been approved by York Region in September 2010 and most recently updated to include approved Official Plan Amendments as of June 1, 2021. The Town of Aurora Official Plan establishes the vision, corresponding principles and supporting policies to guide the Town's evolution and development to the year 2031, for all lands in the Town of Aurora.

The subject property falls within the following Official Plan Amendment Areas:

- Official Plan Amendment No. 34; and,

### 6.6.1 Town of Aurora OPA No. 34 Yonge Street South Secondary Plan

14086 Yonge Street is subject to the Town of Aurora Official Plan Amendment No. 34. OPA 34 was enacted to guide the development of the Yonge Street South area as a “low intensity, environmentally sensitive, primary residential precinct within the Town of Aurora in which new development will occur on the basis of full municipal services.” OPA 34 further emphasizes the provision of “environmentally sensitive forms of development and appropriate management practices which demonstrate a high level of environmental stewardship related to stormwater, groundwater, and protection of sensitive natural features and functions.”

Section 3.2.3 provides density policies within the OPA 34 area, where the following provisions apply:

#### 3.2.3 Density Policies

- a) *The following definitions shall be used in applying the density provisions for residential development within this Secondary Plan area:*
  - i. *"Net residential density" means the number of dwelling units divided by the net residential land area. Net residential land area means the residential land area which excludes all road allowances, schools, places of worship, parks and playgrounds, trails, private open space, environmental features and any use or area not directly related to dwelling units and related area of individual private use; or*
  - ii. *"Gross residential density" means the number of dwelling units divided by the gross residential land area. Gross residential land area includes local roads within the residential area, trails, shared private open space, Environmental Protection and Environmental Function Areas, an elementary school and a local or Neighbourhood Park. Excluded areas include institutional uses such as government offices, a healthcare facility, nursing home, open space associated with a cemetery, or major valley.*
  
- b) *Density transfers for the purpose of clustering residential development in a manner which assists in the preservation or conservation of landscape features such as treed areas, slopes, or other desirable features, may be permitted within and between residential development sites without amendment to this Plan provided that:*
  - i. *land from which density is transferred is deeded to the municipality or subject to a conservation easement, restrictive covenant or other form of legal agreement which ensures that it will remain in an undeveloped and natural form, in perpetuity, continuing its environmental function within the community; and*

- ii. *the resultant building form, location and density of clustered housing is in accordance with the policies, provisions and general intent of this Plan.*
- c) *The gross residential density averaged over the constrained and unconstrained lands subject to a development application shall be no more than 5 units per hectare (2 units/acre).*

### 3.2.4 Compatibility, Buffering and Transitional Density Policies

- a) *In the design of residential developments, measures shall be taken to ensure the appropriate compatibility with nearby uses and environmental features in terms of setbacks or siting, massing, building height, orientation and required landscaping.*
- b) *Residential developments shall have regard for existing and potential sources of noise, odour, visual, traffic and pollution related nuisances arising from any nearby Commercial, Institutional, Open Space, Rural or agricultural land use, as well as existing and potential noise or hazards from transportation facilities including major roads and rail lines.*
- c) *New residential development proposed in areas where adjacent lands have or will have substantially different lot area or density characteristics, will be required to provide significant separation between existing and proposed new buildings. This may be achieved through setbacks, intervening areas of open space, graduated lot areas, landscaping and other means of providing a sensitive transition.*
- d) *Generally more dense development shall be directed towards Yonge Street and lower densities shall form a transition to Estate Residential interfaces,*

### 3.2.5 Typical Infrastructure and Amenities for New Development

*All development in Transitional, Suburban, Cluster or Low Density Residential designations shall be designed so as to incorporate the following:*

- a) *municipal water supply service;*
- b) *municipal sanitary sewer service;*
- c) *Best Management Practices related to storm drainage requirements;*
- d) *paved streets, together with appropriate lighting, walkways and landscaping;*
- e) *any other services, works or considerations that are appropriate for the development or that are required elsewhere in this Plan.*

The subject property is within a “Cluster Residential” land use area regulated under Section 3.6 of OPA 34. Development in Cluster Residential areas is intended to be compatible with its surrounding environmental features.

*Permitted uses are established under Section 3.6.1:*

- *Single Detached Dwellings;*
- *Semi Detached Dwellings;*
- *Linked Housing;*
- *Townhouses; and*
- *Private Open Space.*

Section 3.6.3 further elaborates on development within a Cluster Residential designation, where the following policies apply:

- 3.6.3 Within a Cluster Residential designation, residential units within individual clusters shall be sited in a manner that provides for:
- a) *adequate set back and buffering from environmental features, including conformity with the requirements of any Environmental Impact Study as required in s.11.2.2;*
  - c) *screening by topography and/or vegetation, from existing areas of Estate Residential designation, institutional uses, arterial roads and rail corridors, and maintenance of a minimum separation of 35 metres between any Estate Residential designation and the lot line or limit of any area of private amenity space of any cluster residential unit;*
  - d) *separation from other clusters of residential lots, and from other residential or institutional land use designations, by areas of zoned open space which will remain in common private use or may be dedicated to the municipality;*
  - e) *minimum setback, and screening by topography or vegetation, from Yonge Street in accordance with the Urban Design Guidelines contained in Appendix A.*

The following standards apply to developments within a Cluster Residential land use:

Standard	Requirement	Proposed
<b>Min. Landscaped Open Space</b>	40%	35.72%
<b>Max. Lot Coverage (buildings)</b>	12%	26.17%

Table 2: Aurora Official Plan Cluster Residential Coverage Requirements

Section 8.1 of OPA 34 further covers community identity within the area, where the following provisions apply:

- 8.1.1 *The Yonge Street South community is a distinctive preserve within the Town of Aurora and is intended to retain its low intensity, landscaped character in order to provide visual relief from more intense forms of urban development, and to preserve and enhance the environmental amenity that is provided to the Town and wider region.*
- 8.1.2 *The significant environmental features, natural areas and topography of the Oak Ridges Moraine shall be preserved and enhanced as a fundamental component of the identity of the Yonge Street South area.*
- 8.1.3 *Areas currently developed with large, estate residential lots on private services are intended to be retained and to continue to accommodate low intensity residential use in harmony with the natural environment. New development shall reflect the established heights, massing and landscape quality found in the area and shall be integrated in a sensitive manner within the natural environment and alongside the established residential fabric.*
- 8.1.4 *The area has developed with scattered clusters of development set within environmental and open space preserves. This pattern is intended to continue with local roads providing a linkage to the arterial road system but not intended to provide a continuous route through the community.*

- 8.1.5 *To preserve the quiet character of existing low intensity development, connections shall be discouraged between existing local roads providing direct access to estate lots and new public or private roads or driveways providing access to development which involves a significant number of residential units, a more intense, or a different land use that will introduce increased levels of traffic. Where a connection cannot be avoided, it may be limited to a minor, secondary, or emergency access for new development rather than the principle means of access.*
- 8.1.6 *Connection within the area shall be provided through natural open space areas, linear parks, trails and pedestrian linkages.*
- 8.1.7 *The existing and planned low density and intensity of development is not expected or intended to generate a demand for such additional services within the area as commercial facilities, major institutions, social services, or local transit internal to the area.*
- 8.1.8 *Council does not consider industrial, commercial or intensive institutional facilities to be compatible with the low density, environmentally prominent identity of this portion of the Town due to the visual and functional impact which is characteristic of such land uses, including: required grading, extensive parking and building coverage and related traffic generation.*
- 8.1.9 *Yonge Street is a major gateway to the Town and a central focus for the Yonge Street South community. The height, massing and setback of buildings along Yonge Street, landscaping, streetscape treatment and entrance locations shall be in keeping with the intended character of the area and shall be further described in the Urban Design Guidelines contained in s. 8.3 and further elaborated in Appendix A.*

Section 8.3 outlines policies regulating urban design in the OPA 34 area, where the following provisions apply:

- 8.3.1 *Within the Yonge Street South area, urban design shall emphasize the sensitive visual and functional integration of buildings and structures into the natural setting of the Oak Ridges Moraine in a manner that blends with, protects, and enhances the important features and functions of this significant environment.*
- 8.3.2 *The location, height, size and massing of all proposed infrastructure and development will be designed to be visually unobtrusive and compatible with the landform and landscape character, and with the existing low intensity character of development within the area.*
- 8.3.3 *In order to protect the visual character, amenity and function of the natural environment, controls may be placed on grading; construction and design of driveways, roads, parking areas or pedestrian routes; design of stormwater facilities and related infrastructure, lighting or signage. All these facilities may be limited in location or extent.*
- 8.3.4 *Location of buildings, individual lots or clusters shall be encouraged to complement the natural landscape, vegetation, and adjacent heritage features in addition to respecting the spacing, landscaping and open space amenity of existing development.*
- 8.3.5 *Additional landscaping shall be required:*
- a) *in areas which are deficient in vegetation;*

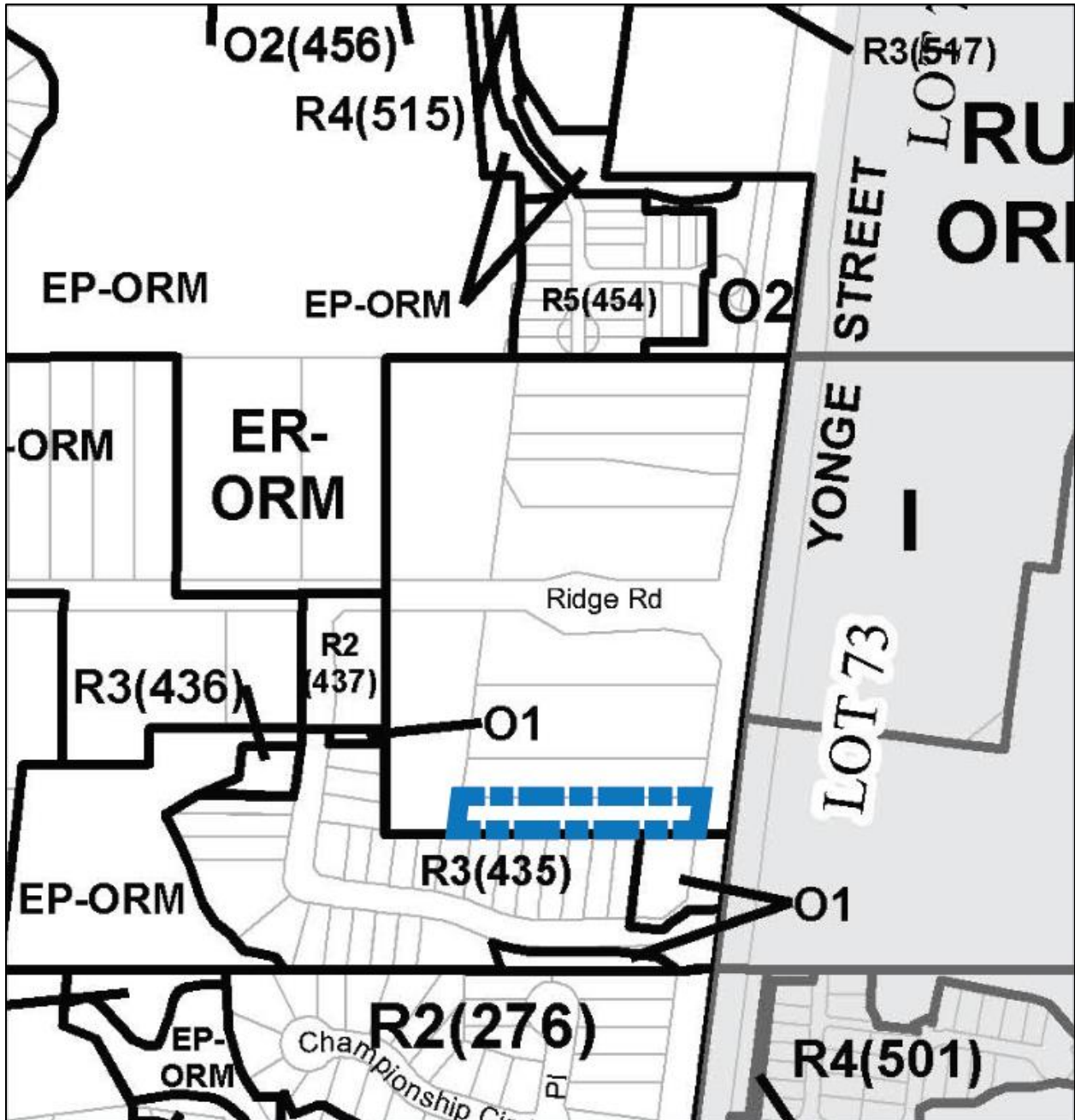
- b) *where buffering is required to screen facilities or more intense land uses;*
  - c) *to augment natural features or functions; or*
  - d) *as a means of providing a landscaped transition from existing areas of lower density.*
- 8.3.6 *To preserve and enhance the distinctive historic, low intensity, environmentally prominent identity of this portion of Yonge Street as the central corridor of the Yonge Street South community and as a principal approach to the more urban core of the Town, buildings shall be set back a minimum of 60 metres from the centerline of the road and other elements of development related to the use and perception of lands within or abutting the roadway will be guided by the provisions of the Urban Design Guidelines contained in Appendix A, including:*
- a) *building height, massing;*
  - b) *landscape treatment; and*
  - c) *location of parking areas, driveways, and access roads.*
- 8.3.7 *In accordance with the Urban Design Guidelines contained in Appendix A, it is Council's intent to provide a distinctive gateway comprised of natural elements on both sides of the intersection of Yonge Street and Bloomington Road to mark this significant entrance into the Town and to the Yonge Street South community.*
- 8.3.8 *In order to facilitate the safe and convenient travel of pedestrians along both sides of Yonge Street, Council in co-operation with the Regional Municipality of York intends to develop a lighted path compatible with the character of the area and sensitive to the environment, in accordance with the more detailed provisions of the Urban Design Guidelines.*
- 8.3.9 *At the intersection of Elderberry Trail and Yonge Street, extensive treed areas shall be retained, and on the southern property, new bermed and landscaped areas created, to ensure that the natural environment continues to form the dominant character at this gateway to the existing estate community.*

The proposed development conforms with the provisions of OPA 34 in the following regards:

- ❖ Proposes a permitted use (townhomes) within the Cluster Residential land use designation;
- ❖ Proposes massing that is compatible with surrounding low density residential land uses in the Yonge Street South area;
- ❖ The Official Plan permits, among others, medium density uses, including townhouse dwellings;
- ❖ The development will provide for an appropriate varied range of residential units comprised predominantly of three (3) bedroom units;
- ❖ Proposes development along Yonge Street – an area where adjacent lots and lands have seen the development of new residential uses of similar densities;
- ❖ Yonge Street is planned to accommodate more dense development at a density that appropriately transitions to the residential neighbourhood to the west;
- ❖ The subject property is within 250 metres of bus stops for the VIVA Blue bus route and the VIVA public transit network;
- ❖ Provides infill development while retaining the low-density residential character of the Yonge Street South community;
- ❖ Does not remove lands from environmental designation, nor disturb KNHFs or KHF;

- ❖ Proposes townhouse dwelling units that do not face Yonge Street, thereby providing unobtrusive design that is compatible with the landform and landscape character of development within the area.

Figure 11 – Aurora Zoning Map  
14086 Yonge Street, Aurora, ON



Source: Town of Aurora Zoning By-law 6000-17 Map

## 6.7 Town of Aurora Zoning By-law 6000-17

The subject property is zoned as “Rural – Oak Ridges Moraine (RU-ORM)” under By-law 6000-17. Sections 12, 14, and 19 apply to the subject property.

Section 12 establishes policies that regulate Rural areas within the Town of Aurora. The permitted uses and required performance standards under Section 12 are as follows:

- Agricultural uses;
- Dwelling, Detached;
- Dwelling, Second Suite;
- Greenhouses;
- Home Occupations;

The performance standards established under Section 12 of By-law 6000-17 include those established under Section 14 for lands zoned RU-ORM, where the standards described under Sections 14 and 19 prevail over those of Section 12, as per Section 12.3.

Standards	RU (Section 12)	ORM (Section 14)	Proposed (Per Unit)
<b>Lot Area</b>	10.0 ha	-	90.78m <sup>2</sup>
<b>Lot Frontage</b>	180.0m	-	6.30m
<b>Front Yard</b>	15.0m	15.0m	1.51m
<b>Rear Yard</b>	22.0m	22.0m	6.50m
<b>Int. Side Yard</b>	9.0m	9.0m & 4.5m	1.52m
<b>Ext. Side Yard</b>	15.0m	15.0m	14.90m
<b>Height (max.)</b>	10.0m	10.0m	12.0m
<b>Lot Coverage (max.)</b>	20%	-	25.76%

Table 3: RU-ORM Building & Lot Standards

Given the proposed building and lot standards, a major zoning by-law amendment application will be submitted as part of the proposal. The Amendment shall seek to rezone the subject property to “Townhouse Dwelling Residential (R8)”, and add some exceptions as per the table below:

Standards	R8 (Section 7)	Proposed (Per Unit)
<b>Lot Area</b>	180m <sup>2</sup>	90.78m <sup>2</sup>
<b>Lot Frontage</b>	30m (6m per unit)	6.30m
<b>Front Yard</b>	7.5m	1.51m
<b>Rear Yard</b>	7.5m	6.50m
<b>Int. Side Yard</b>	0.0m 1.5m (for end units)	0.0m 1.52m
<b>Ext. Side Yard (to Yonge street)</b>	6.0m	14.90
<b>Height (max.)</b>	10.0m	12.0m
<b>Lot Coverage (max.)</b>	50%	25.76%

Table 4: R8 Building & Lot Standards

Section 14 of the Zoning By-law provides general provisions that apply to all lands within the Oak Ridges Moraine Conservation Plan Area, where the following provisions apply to the subject property:

#### 14.1 Settlement Area (-ORM) Permitted Uses

*For lands within the Oak Ridges Moraine Conservation Plan Area.*

*Notwithstanding any other provisions of the By-law to the contrary, within the "Oak Ridges Moraine Settlement Area", as shown on Schedule "A" – Map 1 to this By-law where a Zone symbol is followed by the letters "ORM", no person shall use the land, including expanding, enlarging or otherwise altering an existing use, building or structure, for any use other than a use legally existing as of November 15, 2001, or a use for which a building permit has been issued in accordance with Section 1.9.1 of this By-law, without an amendment to this By-law or relief from this by-law in accordance with the policies of the Official Plan and the Planning Act.*

##### 14.1.2 Settlement Area Lands Within Key Natural Heritage Features

*Despite any other provisions of the By-law to the contrary, where a lot or portion of a lot is located within one or more of the key natural heritage features and/or hydrologically sensitive features identified and shown on Schedule "B", to this By-law, and the said lands are also located within the "Oak Ridges Moraine Settlement Area", as shown on Schedule "A" – Map 1 to this By-law, then the following provisions shall apply:*

- i. the only uses, building and structures permitted on that portion of said lot that is within the feature shall be uses legally existing as of November 15, 2001, or building or structures for which a building permit has been issued in accordance with Section 1.9.1 of this By-law, as amended, and conservation uses;*
- ii. no development or site alteration including expanding, enlarging or otherwise altering an existing use, building or structure shall occur on that portion of said lot that is within the feature, as shown on Schedule "B", to this By-law, without an amendment to, or relief from this By-law, in accordance with the policies of the Official Plan as amended by Official Plan Amendment Number 48 and the Planning Act.*
- iii. on that portion of the said lot that is not located within the feature, the uses permitted shall be in accordance with Section 14.1.1 of this By-law.*

##### 14.4.1 Landform Conservation Areas

*The following provisions shall apply for any application for development or site alteration within a Landform Conservation Area (Category 1) as indicated by cross-hatching on Schedule "E" attached hereto and forming part of this By-law:*

- i. the net developable area of the site that is disturbed shall not exceed 25 per cent of the total site area; and*
- ii. the net developable area of the site that has impervious surfaces shall not exceed 15 per cent of the total site area.*

The proposed development generally conforms with the provisions of Zoning By-law 6000-17 in the following regards:

- ❖ Proposes a zoning by-law amendment application to rezone the subject property and to add site specific exemptions to facilitate the development of 20 townhomes;

- ❖ Ensures that future development on other lots in the area will be required to undergo the zoning by-law amendment process to provide appropriate residential intensification;
- ❖ Proposes a 1.80m privacy fence to minimize overlook upon abutting properties resulting from 2m excess height and 6.5m rear yard setbacks;
- ❖ Proposes the construction of a private right of way, ensuring that new units do not front Yonge Street thereby easing the visual impact of reduced frontages and front yards;
- ❖ The proposed development maintains and is generally consistent with the intent of the proposed “R8” Zone classification for the subject property. The proposed Zoning By-law Amendment is minor and will not set a precedence for new development that is out of scale within the surrounding area as this area is intended for growth as dictated by both the Regional and local Official Plans. In this instance, the proposed lot pattern and built form, via a private condominium laneway, will facilitate a much more compact built form effectively utilizing the overall configuration and massing of the subject lands;
- ❖ The changes in zoning and proposed residential use will effectively integrate with the surrounding community that will assist with the evolutionary transformation from the currently underutilized single-detached dwelling of the lands to a thriving, complete and compact residential community; and,
- ❖ Proposes a 14.90m setback from Yonge Street, and a varied landscaped strip (ranging from 2m to 5m in width) along the property’s frontage with Yonge Street, further reducing the visual impact of the proposed densification.

## 6.8 Town of Aurora Green Standards

A fulsome Green Standards summary shall be provided to the Town of Aurora at such a time where this development application has reached the Site Plan stage.

The proposal consists of a 20-unit residential townhouse development each of three (3) storeys in height and with GFAs of approximately 2,160 square feet. Each unit is designed based on a family oriented concept, with three (3) bedrooms, an open-plan kitchen and living room, as well as an underground parking garage with two (2) parking spaces per unit.

The design has been carefully considered to align with the Town of Aurora's Official Plan and Urban Design Guidelines to ensure design excellence, promote responsible growth management, and building a greener community.

The proposed development shall positively contribute to the Town of Aurora by adding affordable budget friendly family homes that provide space and privacy for each unit, with the benefit of low maintenance. This desirable location is within proximity to large communities, such as Aurora and Richmond Hill, and within walking distance to public transit.

This development application has been revised, and has been discussed with the Town of Aurora to ensure that the project meets all necessary standards and positively impacts the community.

## 7.0 Planning Justification & Conclusion

The proposal seeks to provide gentle residential densification in an area that is experiencing, and able to accommodate considerable intensification. The proposal seeks to develop 20 residential townhouse dwelling units in three blocks, accessed via a private right-of-way connecting to Yonge Street. This density is similar to other residential development projects in the area, as per Section 3.3 of this report, where multiple plans of subdivision, townhome developments, and mid-rise developments have been proposed and approved in the area. The subject property is fully municipally serviced with immediate access to public transit services, connecting residents to the York Region Transit network through the VIVA Blue bus route.

The subject property is located within a settlement area, a 'Built-Up Area' under the Growth Plan, and fronts a major arterial road with existing transit services. The application furthermore conforms with the associated Secondary Plan (Town of Aurora Official Plan Amendment No. 34), which direct residential development to the Yonge Street South area.

This proposal ultimately seeks to facilitate appropriate and compatible residential intensification that maintains the low-rise character of the Yonge Street South neighbourhood to support the Town of Aurora's projected growth into 2031. The development of townhomes will optimize the subject property's land use efficiency, ensure that abutting lands will have the ability to be developed in the future without undue encumbrance, and ensure that residents are fully supported by existing and planned municipal services and utilities – thereby contributing towards the development of a compact, complete and accessible communities.

The proposed development respects the policy initiatives of the Provincial Policy Statement, the Places to Grow Act, the Region of York Official Plan, the Town of Aurora Official Plan and the Town of Aurora Zoning By-law 6000-17.

The proposed development application has been prepared in the context of the existing municipal and provincial planning policy framework, is based on good land use planning principles and should proceed through the planning process under the Planning Act.

## 8.0 Professional Studies Submitted by the Applicant

### 8.1 Natural Heritage Evaluation Addendum Analysis

Azimuth Environmental Consulting Inc. was retained by Esmail Zameni to conduct a Natural Heritage Evaluation for the proposed development on the subject lands.

The original report was prepared on March 1, 2017. At that time, the proposed redevelopment involved construction of a medical building. An updated NHE/EIS report was issued on April 13, 2021, based on a revised Site Plan involving construction of a single residential dwelling on each of five (5) lots within the property limits and the most current revision is dated April 2024. The current proposed redevelopment involves construction of twenty (20) townhouse units. It is understood that the 20 townhouse units will occupy the same overall development footprint as the previously proposed five (5) residential dwellings on the property.

The Addendum includes responses to agency review comments in the form of a Comment Matrix, the new Site Plan (Appendix A), updated Figures to reflect the new redevelopment proposal and updated background information, as required (Appendix B).

In summary, the combination of the above reports provides that *“the proposed development conforms to most relevant policies related to key terrestrial natural heritage features and functions conveyed through the PPS, ORMCP and municipal documents reviewed within the scope of this report.”*

As discussed, please note that agency approvals *may* require completion of some additional fieldwork and assessments. If necessary, once the additional requested fieldwork and assessments have been completed (separate scope), during the site plan application stage, Azimuth can provide any necessary future addendum analysis to the Comment Matrix (separate scope).

Azimuth will provide an updated Phosphorus Report under separate cover.

### 8.2 Phase One Site Assessment

Orbit Engineering Limited (Orbit) was retained to carry out a Phase One Environmental Site Assessment (ESA) for the proposed development- referred to as the “site” and “Phase One Property”).

The Phase I ESA made the following conclusions:

- The Phase One Property is throughout used for residential purposes. No PCAs and no APECs were identified for this Phase One ESA.
- The historic and current uses of the properties adjacent to the Site do not pose an environmental concern.
- A Phase Two ESA is not recommended.

### 8.3 Traffic Impact Study

Asurza Engineers Ltd. was retained to conduct a Traffic Brief in support of the proposed townhouse development.

The purpose of this analysis is to assess whether the proposed development will have any significant impacts on the traffic operations of the adjacent road.

The Study found that due to the very limited scale of the proposed development, the additional trips will impose virtually no impact on the adjacent roads. Any minor traffic impact as a result of the new trips will be negligible to the current traffic operations.

#### 8.4 Stormwater Management Report

Land & Building Experts has been retained by the owner to prepare a Stormwater management report for the proposed development.

The preceding sections and the detailed design analysis appended herewith indicate that the proposed stormwater management system meets with the Town of Aurora requirements.

	2 year	5 year	10 year	25 year	50 year	100 year
Post-development flow rate (m <sup>3</sup> /s)	0.0233	0.0321	0.0380	0.0442	0.0515	0.0573
Predevelopment flow rate (m <sup>3</sup> /s)	0.0233	0.0321	0.0380	0.0442	0.0515	0.0573
Required storage (m <sup>3</sup> )	24.56	33.97	40.73	48.26	55.48	61.00
Available storage (m <sup>3</sup> )	65.80	65.80	65.80	65.80	65.80	65.80

Stormceptor EFO4 has been recommended to address quality control requirements.

It is therefore recommended that this report be adopted for detailed design if the Town of Aurora finds the analysis noted herein acceptable.

#### 8.5 Archaeological Assessment

Earthworks was retained in 2021 to conduct a Stage 2 archaeological assessment of the property located at 14086 Yonge Street, Aurora. The survey was completed under PIF #: P321-0287-2022 and resulted in no archaeological sites located and no further archaeological assessments recommended. The Stage 2 archaeological assessment report was entered into the Register of Archaeological Reports by the Ministry of Heritage, Sport, Tourism, and Culture Industries on October 21, 2021.

Earthworks was provided with a revised site plan on November 21, 2023. Upon review, Earthworks has determined that the boundaries of the proposed development have not changed from the original Stage 2 assessment, and the conclusions of no further archaeological work being required are still in place.

#### 8.6 Transportation Study

Asurza Engineers Ltd. was retained to conduct a Traffic Brief in support of the proposed townhouse development as well as provide a Traffic Management Plan.

The purpose of this traffic management plan is to provide guidance for construction works happening in close proximity to regional roads.

The Study made the following conclusions:

- *The construction traffic resulting from the proposed townhouse development located at 14086 Yonge St, Town of Aurora, can be accommodated without significantly impacting on the safety and traffic operations of the adjacent roads.*

## 8.7 Preliminary Hydrogeological Assessment

GeoPro Consulting Limited (“GeoPro”) was retained to conduct a hydrogeological site assessment (including water balance assessment) for the proposed townhouse development.

The findings of the analysis are summarized as per follows:

### PRELIMINARY RECOMMENDATIONS/CONCLUSIONS

#### 7.1 Water Discharge

*Considering the relatively deep-water table on the Site, temporary dewatering may not take place. Therefore, significant volume of water discharge would not be anticipated. However, should there be water accumulation due to the precipitation during the excavation and installation, the water may be collected and discharge overland.*

#### 7.2 Impacts due to Dewatering Activities

*Given that forced dewatering and water drainage from likely sub-drainage systems may not occur, impacts on the adjacent structures, water wells, and surface water would not be anticipated for the proposed development.*

#### 7.3 Monitoring Well Decommissioning

*According to Ontario Regulation 903 (“O. Reg. 903”), when the monitoring wells are no longer used, they should be decommissioned by a licensed water well contractor.*

## 8.8 Functional Servicing Report

Land & Building Experts has been retained by the owner to prepare a Functional Servicing Report for the proposed development. This report describes how the servicing plan satisfies the objectives for the site.

The purpose of this report is to provide a conceptual framework for servicing the proposed townhouse with respect to water supply, sanitary sewage, and storm drainage.

The report made the following conclusions:

*The above analysis indicates that the proposed development shall be provided with sanitary, storm, and water services without any constraint.*

*It is our opinion that adequate services exist to support the proposed addition at 14086 Yonge Street. Recommendations from our report shall be incorporated into the detailed design phase of the addition following the approval of the site plan.*

## 8.9 Phosphorus Assessment Update

Azimuth Environmental Consulting Inc. was retained to undertake a Phosphorus Assessment for the development. The evaluation focused on the existing phosphorus budget at the subject property and the potential for the proposed development to impact the existing conditions. Azimuth Environmental Consulting Inc. made the following conclusions in their report:

It is our understanding of the report that the subject property will be developed into a twenty (20) unit residential townhome block with associated access road, parking, and landscaped areas. The purpose of this Phosphorus Assessment is to evaluate the net impact from the proposed development and the effectiveness of BMPs and LIDs that will reduce phosphorus loads.

A geotechnical and hydrogeological evaluation was completed for the Site in July of 2020 by GeoPro. All 4 boreholes were completed with ground water monitoring wells to a maximum 6.1 mbgs with no ground water being encountered during the July 2020 monitoring event. Soils consisted of topsoil overlying reworked Silty Fine Sands and Fine Silt (extending to depths of 1.2 mbgs) overlying Sandy Silt, Fine Sand and Silt, Silty Fine Sand and Fine Sand to 6.6 mbgs. No bedrock was encountered.

According to local topographic mapping, the Site is found at an elevation between 144 – 149 masl and displays a gradual slope to the west. The Site is located within the East Holland sub-watershed of the Lake Simcoe Watershed. Based on the topography of the Site and surrounding areas, the shallow ground water is likely to flow northerly towards the Tannery Creek and Lake Simcoe.

A phosphorus budget was completed using the MECP's Phosphorus Budget Tool. The pre-development phosphorus load was calculated to be 0.05 kg/yr based on 0.4 ha of low intensity development (i.e., rural development) land use. The post-development phosphorus load without LIDs was calculated to be 0.53 kg/yr based on a high intensity (i.e., residential) development. A Functional Servicing and Stormwater Management Report completed by Alpin and Martin (2020) included details related to phosphorus removal measures. These included incorporation of an underground storage system with infiltration to collect the driveway, 80% of the landscaped area, and parking areas as well as soak away pits to collect all rooftop. Both phosphorus removal strategies have an efficiency rating of 70% (detention pit with infiltration) and 60% (soak away pit) which are derived from the values given in Hutchinson (2012). These LIDs would reduce the post-development phosphorus load by 50% to 0.27 kg/yr, which would be an increase of 0.22 kg/yr over the Site scale.

## 8.10 Environmental Noise Assessment

YCA Engineering Limited was retained to prepare an updated Environmental Noise Assessment for the proposed development.

*The report made the following conclusions and recommendations:*

1. *Mandatory air conditioning is required for Block 1 (Unit 1).*
2. *Provision for adding air conditioning is required for Block 1 (Units 2 to 10) and Block 2 (Units 11 to 13).*
3. *Standard windows and exterior wall constructions meeting the OBC requirements are sufficient in order to meet the indoor sound levels.*
4. *For Block 1 (Unit 1), a 2.5m high acoustic fence is required as shown on the attached Figure 2 to reduce the sound level at the outdoor amenity area (back yard) to 55dBA. The acoustic barrier requirements should be reviewed once the detailed/final grading information are available.*
5. *All applicable warning clauses shall be listed in the Town of Aurora's Development Agreement and also be inserted in the Agreements of Purchase and Sale or Lease and registered on title.*
6. *Prior to construction of the buildings and dwelling units, it is recommended that the architectural plans be reviewed to ensure all building components, ducting systems and air conditioning locations meet the acoustical requirements.*

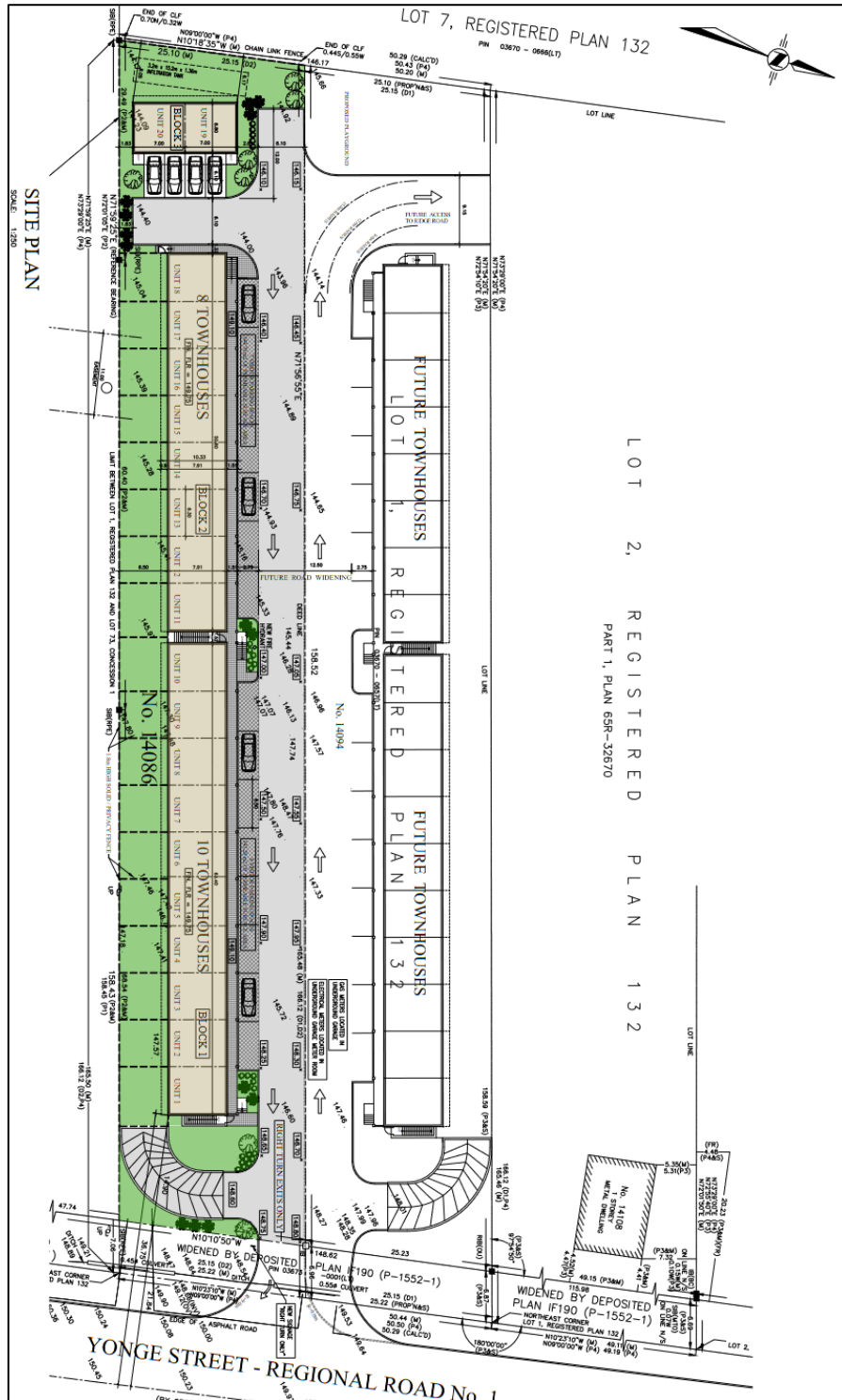
7. *Prior to the issuance of occupancy permits, the Town's building inspector or a Professional Engineer qualified to perform acoustical engineering services in Ontario shall certify that the noise control measures have been properly installed and constructed.*

#### **CONCLUSION**

*This report has determined that sound levels acceptable to the Ministry of Environment, Conservation and Parks, Town of Aurora and the Region of York are expected to be achieved using the abatement measures in this report and as shown on the attached Figure 2.*



Figure 13 – Conceptual Site Plan  
14086 Yonge Street, Aurora, ON



**Figure 14 – Proposed Floor Plans**  
**14086 Yonge Street, Aurora, ON**

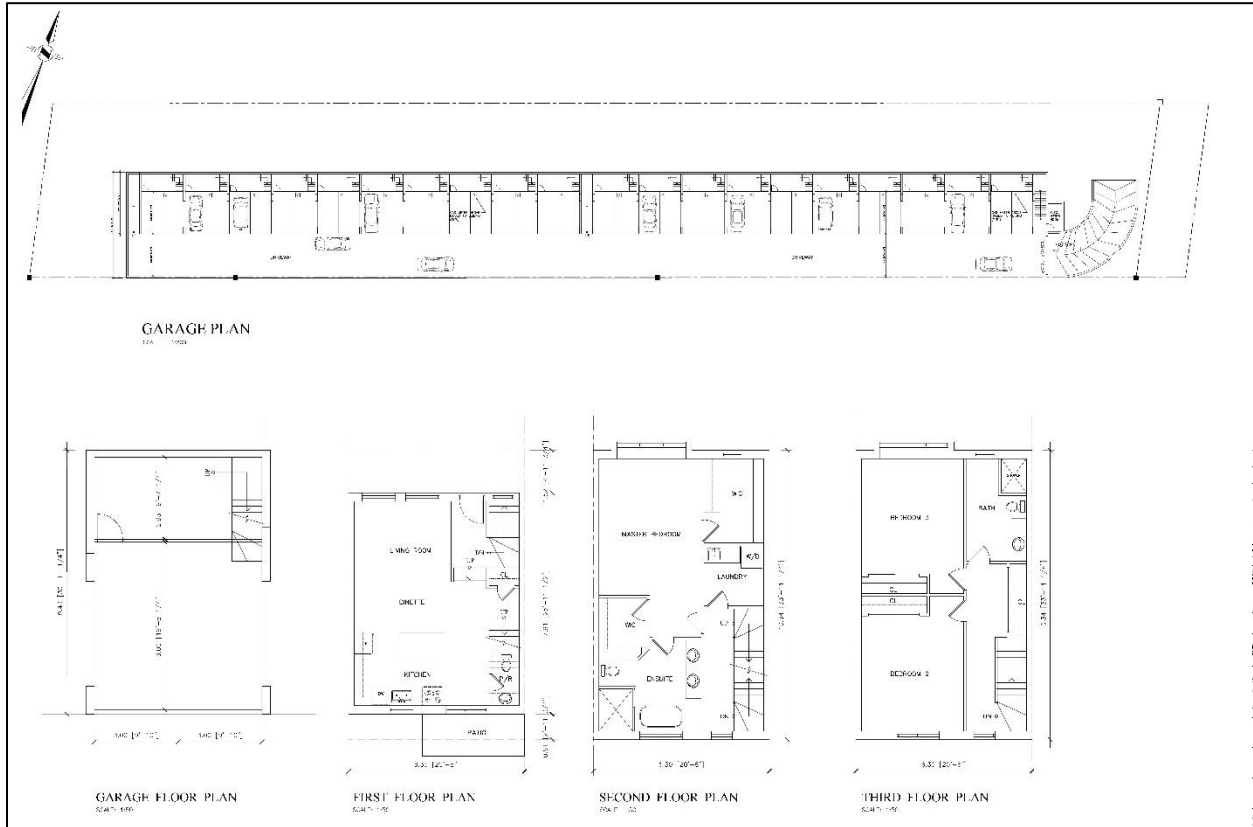
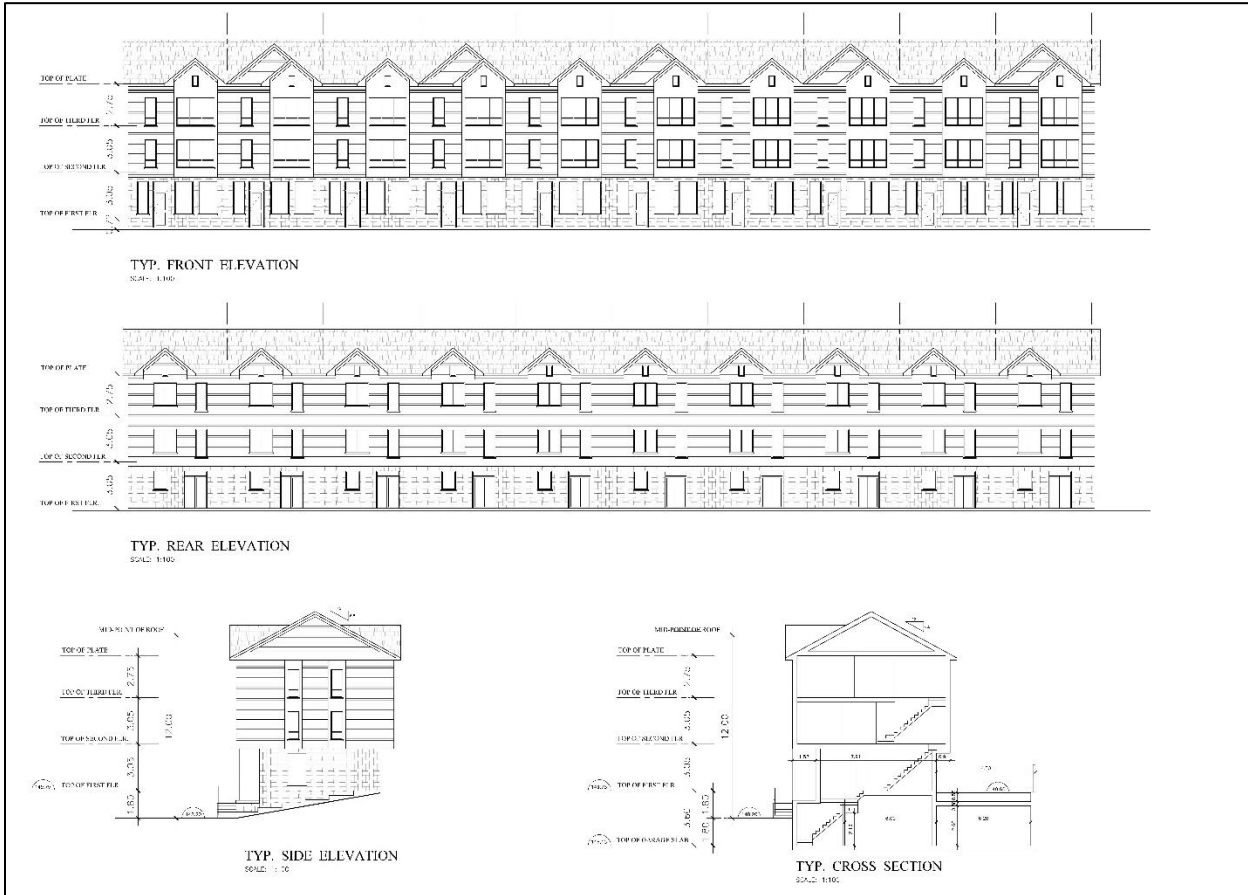


Figure 15 – Elevations & Cross-Section  
14086 Yonge Street, Aurora, ON



**Figure 16 – Landscape Plan**  
**14086 Yonge Street, Aurora, ON**

