



**Trails and Active
Transportation
Committee
Meeting Agenda**

Friday, June 15, 2018

10 a.m.

Council Chambers

Aurora Town Hall

Public Release
June 8, 2018



**Town of Aurora
Trails and Active Transportation Committee
Meeting Agenda**

Date: Friday, June 15, 2018

Time and Location: 10 a.m., Council Chambers, Aurora Town Hall

1. Approval of the Agenda

Recommended:

That the agenda as circulated by Legislative Services be approved.

2. Declarations of Pecuniary Interest and General Nature Thereof

3. Receipt of the Minutes

Trails and Active Transportation Committee Meeting Minutes of April 20, 2018

Recommended:

That the Trails and Active Transportation Committee meeting minutes of April 20, 2018, be received for information.

4. Delegations

5. Matters for Consideration

**1. Memorandum from Senior Landscape Architect
Re: Trailhead Parking – BG Properties (Aurora) Inc.**

Recommended:

1. That the memorandum regarding Trailhead Parking – BG Properties (Aurora) Inc. be received; and
2. That the Trails and Active Transportation Committee recommend to Council:
 - (a) That Option B for the Trailhead Parking Area be approved.

**2. Memorandum from Manager of Parks and Fleet
Re: Wellington Street East Crossing Design Concepts**

Recommended:

1. That the memorandum regarding Wellington Street East Crossing Design Concepts be received; and
2. That the Trails and Active Transportation Committee recommend to Council:
 - (a) That Option 4 for the Wellington Street East crossing be approved.

6. Informational Items

**3. Memorandum from Senior Landscape Architect
Re: Trailhead Parking – 46 St. John's Sideroad**

Recommended:

1. That the memorandum regarding Trailhead Parking – 46 St. John's Sideroad be received for information.

**4. Verbal Update from Analyst, Traffic/Transportation
Re: Hartwell Way Stop Signs**

Recommended:

1. That the verbal update regarding Hartwell Way Stop Signs be received for information.

**5. Verbal Update from Manager of Parks and Fleet
Re: Wildlife Park and 2C Maps**

Recommended:

1. That the verbal update regarding Wildlife Park and 2C Maps be received for information.

**6. Verbal Update from Senior Landscape Architect
Re: BG Trail Adjacent to Rail and Cattle Crawl**

Recommended:

1. That the verbal update regarding BG Trail Adjacent to Rail and Cattle Crawl be received for information.

7. New Business

8. Adjournment



**Town of Aurora
Trails and Active Transportation Committee
Meeting Minutes**

Date:	Friday, April 20, 2018
Time and Location:	10 a.m., Council Chambers, Aurora Town Hall
Committee Members:	Councillor Tom Mrakas (Chair), Councillor Sandra Humfries (arrived 10:42 a.m.), Alison Collins-Mrakas, Richard Doust (PRCSAC Representative), Laura Lueloff, and Nancee Webb (EAC Representative)
Members Absent:	Bill Fraser
Other Attendees:	Sara Tienkamp, Parks Manager, Gary Greidanus, Senior Landscape Architect, Lawrence Kuk, Senior Planner, Michael Bat, Transportation Analyst, and Linda Bottos, Council/Committee Secretary

The Chair called the meeting to order at 10:06 a.m.

1. Approval of the Agenda

**Moved by Laura Lueloff
Seconded by Richard Doust**

That the agenda as circulated by Legislative Services, with the following addition, be approved:

- Delegation (a) Ron Weese; Re: Proposed Open Trails Event

**On a two-thirds vote the motion
Carried**

2. Declarations of Pecuniary Interest and General Nature Thereof

There were no declarations of pecuniary interest and general nature thereof under the *Municipal Conflict of Interest Act, R.S.O. 1990, c. M.50.*

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3. Receipt of the Minutes

Trails and Active Transportation Meeting Committee Minutes of November 17, 2017

**Moved by Nancee Webb
Seconded by Richard Doust**

That the Trails and Active Transportation Committee meeting minutes of November 17, 2017, be received for information.

Carried

4. Delegations

**(a) Ron Weese, Chair and Executive Director, Activate Aurora
Re: Proposed Open Trails Event**

Mr. Weese presented a brief overview of a proposed concept to raise the awareness and understanding of the Town's trails system through a variety of activities including cycling, such as inspirational and educational programs, and interpretive studies. He invited members of the Committee to consider working with the Activate Aurora team in developing this new event.

**Moved by Nancee Webb
Seconded by Laura Lueloff**

That the comments of the delegation be received for information.

Carried

5. Matters for Consideration

**1. Memorandum from Senior Landscape Architect
Re: Trailhead Parking – BG Properties (Aurora) Inc.**

Staff provided an overview of the memorandum and Concept Plan options for the location of the Trailhead Parking Area.

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The Committee and staff reviewed and discussed the pros and cons of the location options. Staff agreed to provide further detailed information and arrange for communications with residents.

Moved by Nancee Webb

Seconded by Richard Doust

1. That the memorandum regarding Trailhead Parking – BG Properties (Aurora) Inc. be received; and
2. **That the comments of the Trails and Active Transportation Committee be received and referred to staff for consideration and action as appropriate.**

Carried as amended

6. Informational Items

2. Extract from Council Meeting of January 30, 2018

Re: Trails and Active Transportation Committee Meeting Minutes of November 17, 2017

Moved by Alison Collins-Mrakas

Seconded by Laura Lueloff

1. That the Extract from Council Meeting of January 30, 2018, regarding the Trails and Active Transportation Committee meeting minutes of November 17, 2017, be received for information.

Carried

3. Extract from Council Meeting of February 13, 2018

Re: Summary of Committee Recommendations Report No. 2018-01; Re: Trails and Active Transportation Committee Meeting Minutes of November 17, 2017

Moved by Richard Doust

Seconded by Nancee Webb

1. That the Extract from Council Meeting of February 13, 2018, regarding Summary of Committee Recommendations Report No. 2018-01; Re: Trails

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and Active Transportation Committee meeting minutes of November 17, 2017, be received for information.

Carried

7. New Business

The Committee inquired about the plans for a status report regarding the proposed trail crossing on Wellington Street East at John West Way. Staff provided an update and noted that further details from the consultant may be available for the June Committee meeting.

The Committee inquired about the status of the trails system in the 2C lands and suggested that area residents be kept informed. Staff provided an update and noted that project status updates could be made available on the Town's website.

The Committee noted that many drivers are failing to stop at stop signs in the Hartwell Way bridge area, noting that it is a high traffic area for children attending Rick Hansen Public School. The Committee expressed safety concerns and suggested that lines be painted on the road to indicate pedestrian crossings and raise awareness of the stop signs. Staff agreed to investigate the matter and report back to the Committee.

The Committee referred to the delegation by Ron Weese regarding a proposed Open Trails event, and Nancee Webb and Laura Lueloff volunteered to assist the Activate Aurora team in the development of the event.

The Vice Chair noted that the grades six and seven students from Regency Acres Public School have designed a dog park for the Highland Gate lands, and the students plan to present their proposal to Council and a future Committee meeting.

The Chair referred to a recent Application for Draft Plan of Subdivision for the Stronach South Wellington Lands, which was considered at the March 2018 Public Planning meeting, and inquired about the opportunity for the Committee to provide input to the development of the trails within the application. Staff indicated that information should be available for the June Committee meeting.

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8. Adjournment

Moved by Councillor Humfryes

Seconded by Richard Doust

That the meeting be adjourned at 11:01 a.m.

Carried

Committee recommendations are not binding on the Town unless otherwise adopted by Council.



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**Town of Aurora
Operational Services
Parks Division**

Memorandum

Date: June 15, 2018
To: Trails and Active Transportation Committee
From: Gary Greidanus, Senior Landscape Architect
Re: **Trailhead Parking – BG Properties (Aurora) Inc.**

Recommendation

- 1. That the memorandum regarding Trailhead Parking – BG Properties (Aurora) Inc. be received; and**
- 2. That Option B for the Trailhead Parking Area be approved.**

Background

At the Trails and Active Transportation Committee Meeting of April 20, 2018 the Committee requested additional information including the pros and cons regarding the proposed Trailhead Parking options within the BG Properties (Aurora) Inc. (Woodhaven Aurora) development. Both Trailhead Parking options “A” and “B” locations are identified on the Woodhaven Aurora Neighbourhood Plan – Attachment 1.

Trailhead Parking Option “A” involves the use of an existing shared-use driveway accessing Yonge Street at the north end of the Woodhaven development and effects two properties, the ‘Ren’ property to the north and the ‘Coutts’ property to the south. (Please refer to Block F, OPA 34 Town of Aurora Ownership Plan – Attachment 2) In this scenario, an easement would be required with the Ren property to facilitate the shared-use driveway. Staff have contacted the Owners of the Ren property and they are amenable to the easement and the design of the Trailhead Parking on the adjacent Coutts property. (Please refer to North Trailhead Parking Area – Attachment 3). The Coutts property would then be acquired by the Town in either one of two ways:

Trailhead Parking – BG Properties (Aurora) Inc.
June 15, 2018

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- BG Properties (Aurora) Inc. would purchase the Coutts property, complete the design and construction of the parking area and trail, and dedicate the entire land area to the Town to satisfy the parkland requirement of the Woodhaven development;
- Alternatively, the Town could purchase the Coutts property and complete the design and construction. In this scenario BG Properties (Aurora) Inc. would satisfy its parkland requirements by paying cash-in-lieu to the Town based on lot values at the time of building permit availability.

The Coutts property is approximately 16 acres and is zoned EP – Environmental Protection. An accredited appraiser would need to complete a report and establish the final purchase/sale value. An informal appraisal, without any analysis, suggests that the property value is in the \$2,400,000.00 range.

Cash in lieu of parkland has not been determined at this time and will require an up to date appraisal. However, if 2017 appraisal values for Phase 1 of this development were applied to this current phase of development the Town would receive an estimated \$5,600,000.00 contribution in cash-in-lieu of parkland. Please note this is an approximate estimate only and the actual figures would need to be based on an appraisal prior to building permits for Phase 2.

Trailhead Parking Option “B” is the proposal to locate the parking lot within the emergency access to Ridge Road at the south end of the development (Please refer to Trailhead Parking Area Concept Plan – Option B, Attachment 4). This concept proposes in-line parking stalls parallel to the emergency access road travel lane. The emergency access road would need to be conveyed to the Town to facilitate this option.

The pros and cons associated with Trailhead Parking Option “A” and Option “B” is addressed in the following Table 1.

Trailhead Parking – BG Properties (Aurora) Inc.
June 15, 2018

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Table 1

Trailhead Parking Options	Pros	Cons	Approximate Construction Costs
Trailhead Parking Option “A” (North Trailhead)	<ul style="list-style-type: none"> • Direct access to pet cemetery. • Direct access off of Yonge Street. • Good connectivity to Yonge Street Transit • Acceptable to neighbour • Purchase of land will provide additional 16 acres of public open space. 	<ul style="list-style-type: none"> • Significant costs associated with purchase of private land and/or not receiving full cash-in-lieu of parkland. • Construction costs higher than Option “B” 	+/- \$180,000.00
Trailhead Parking Option “B” (Parking Emergency Access)	<ul style="list-style-type: none"> • Does not require significant land purchase. • Town will receive entire cash-in-lieu of parkland. 	<ul style="list-style-type: none"> • Local residents may object to access road being used for public parking. • Public use and privacy concerns from adjacent land owners. • Parallel parking in emergency access route is not ideal and would require a turn-around area. • Indirect connection to Pet Cemetery. • Further away from Pet Cemetery than Option “A”. 	+/- \$75,000.00

Trailhead Parking – BG Properties (Aurora) Inc.
June 15, 2018

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		<ul style="list-style-type: none">• Poor connectivity to Yonge Street Transit.	
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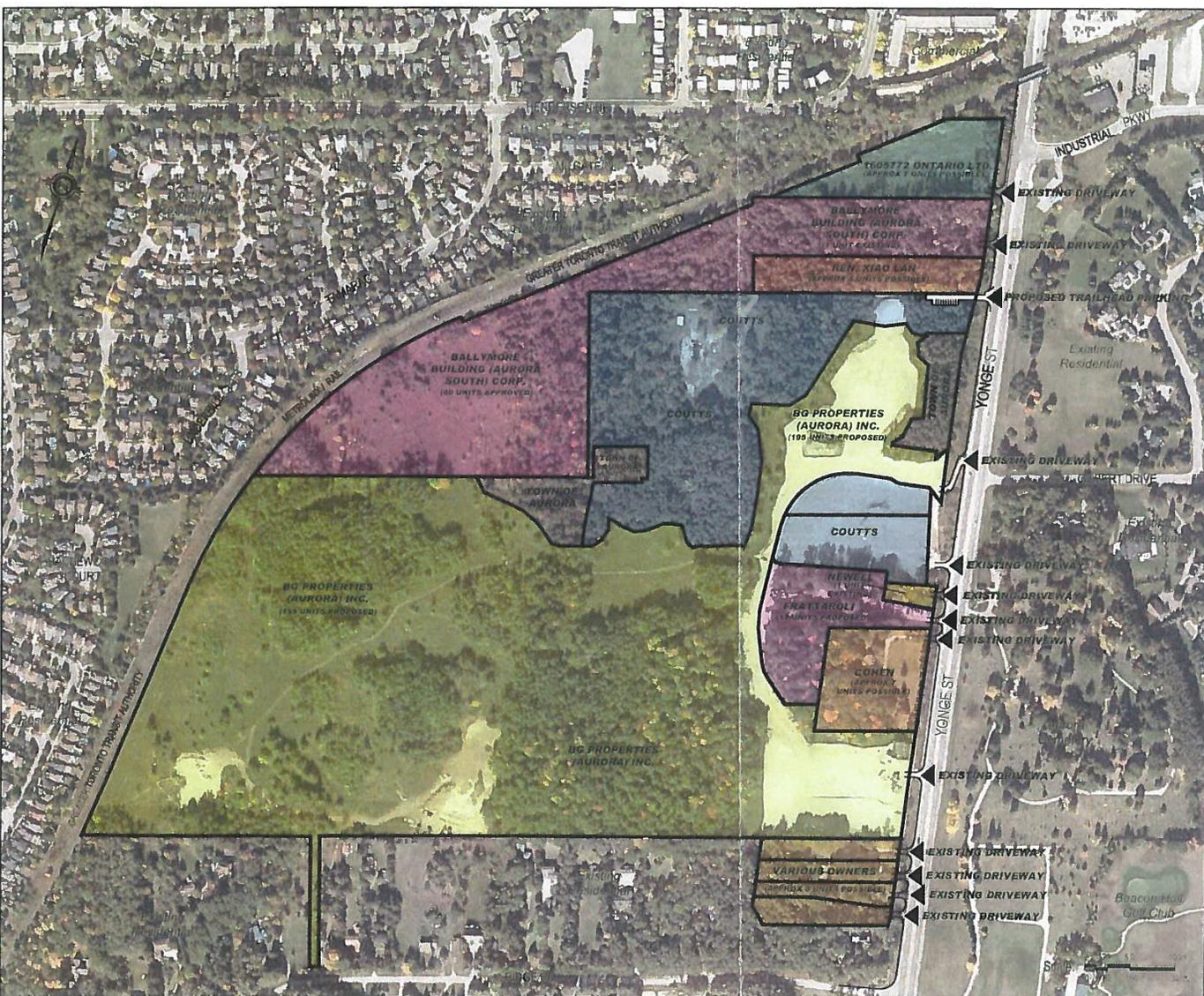
Attachments

Attachment 1 – Woodhaven Aurora Neighbourhood Plan

Attachment 2 – Block F, OPA 34 Town of Aurora Ownership Plan

Attachment 3 – North Trailhead Parking Area (Option A)

Attachment 4 – Trailhead Parking Area Concept Plan – Option B



**BLOCK F, OPA 34
 TOWN OF AURORA
 OWNERSHIP PLAN**

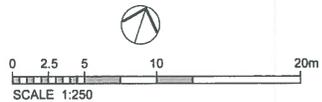
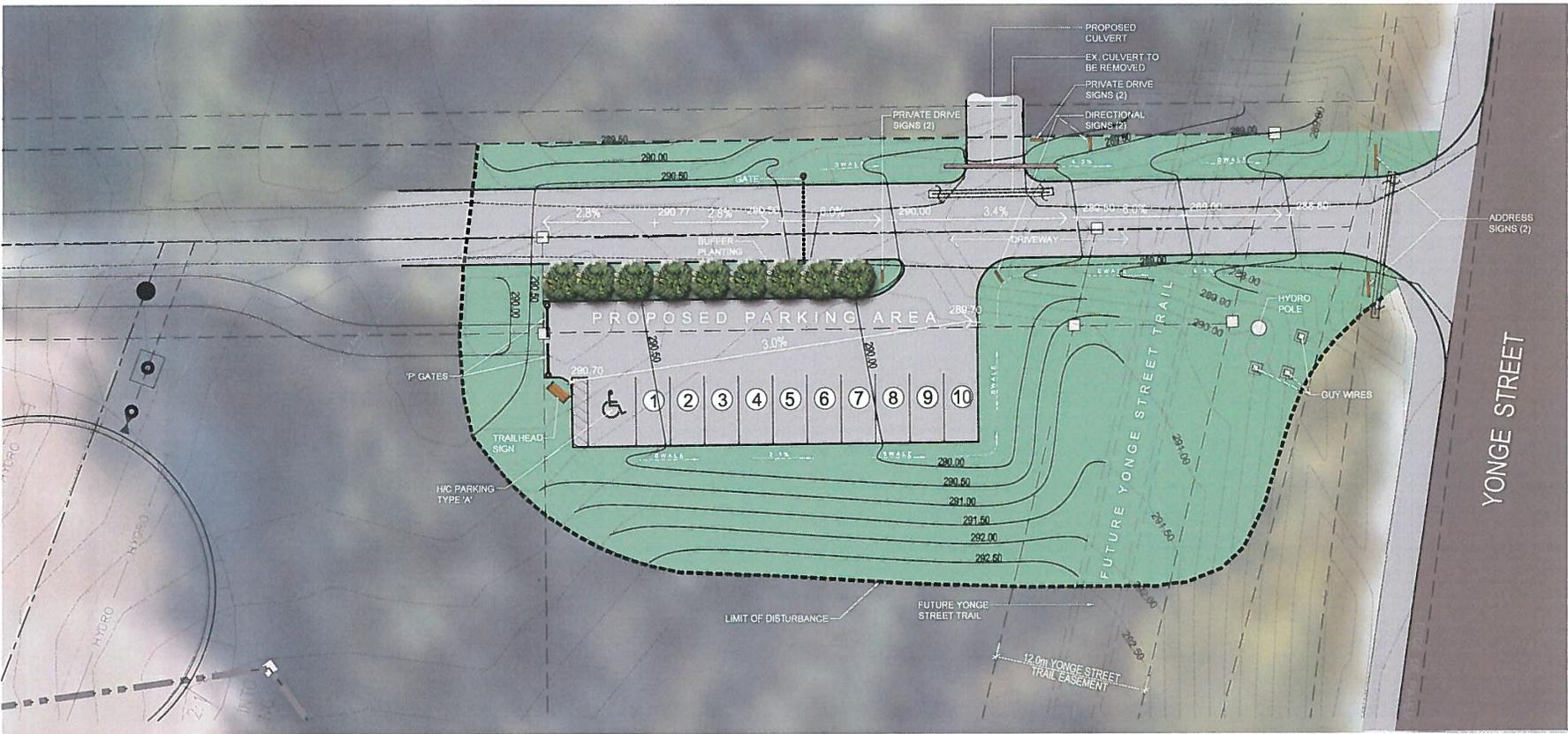
Note: Approximate units possible determined by max 5 units per hectare (Town of Aurora OPA No. 34, Policy 3.2.3.c.).

Date: October 19, 2017
 Project No.: 14-2274 & 14-2283
 Prepared by:

MALONE GIVEN PARSONS LTD.

Attachment 2

Attachment 3



BG PROPERTIES (AURORA) INC
 NORTH TRAILHEAD PARKING AREA



SCHOLLEN & Company Inc.
 30 Waukena Court, Unit 115
 Richmond Hill, Ontario L4B 1B3
 Tel: 905-885-0077
 Fax: 905-885-0010



100 John West Way
Box 1000
Aurora, Ontario
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www.aurora.ca

**Town of Aurora
Operational Services
Parks Division**

Memorandum

Date: June 15, 2018
To: Trails and Active Transportation Committee
From: Sara Tienkamp, Manager Parks and Fleet
Re: **Wellington Street East Crossing Design Concepts**

Recommendation

- 1. That the memorandum regarding Wellington Street East Crossing Design Concepts be received; and**
- 2. That Option 4 for the Wellington Street East crossing be approved.**

Background

The Trails Master Plan identifies a potential trail crossing at Wellington Street East, to allow trail users direct access from the Tim Jones Trail north to the south leg of the Tim Jones Trail across Wellington Street East, just east of John West Way.

In 2017 the Parks staff retained the services of Dillon Consulting to undertake a feasibility study to provide preliminary plans and cost estimates for a multi purpose barrier free trail crossing. Initial meetings identified four (4) options:

- Option 1: Above Grade Crossing (overpass bridge)
- Option 2: Below Grade Crossing (underpass tunnel)
- Option 3: At Grade Level Crossing (signalized mid-block pedestrian crossing)
- Option 4: Do nothing

Wellington Street East Crossing Design Concepts
June 15, 2018

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The four options were presented to the Region of York and Lake Simcoe Region Conservation Authority (LSRCA). The Region of York advised that they would not accept Option 3: At Grade Level Crossing, as the proposed location is in too close of proximity to the existing designated crossing at Bayview Avenue and John West Way. However, both the Region of York and LSRCA were both amenable to exploring the below grade and above grade opportunities.

Dillon Consulting has prepared preliminary report including the advantages and disadvantages as well as developed concept drawing and costing for the overpass and underpass options (attached)

The costs associated with both Option 1 and Option 2 are significant. The overpass bridge is estimated at approximately \$10.3 million and the underpass tunnel \$3.3 million. While the tunnel is optimal as it keeps trail users within the trail system and in the valley, costs to construct are considerable. Option 4 suggests blocking the existing trail (south of Wellington) with a barrier to prevent access and naturalizing the trail with trees/shrubs. In this option trail users would be lead to the west along the existing trail up to John West Way where they would utilize the traffic lights at John West Way and Wellington St E to cross to the south side of Wellington St to gain trail access to the Tim Jones Trail south.

Attachments

Attachment 1 – Wellington Street East Pedestrian Crossing Study – Preliminary Findings



MEMO

TO: Sara Tienkamp
FROM: Paul MacLeod
cc: Chad Carreras
DATE: June 06, 2018
SUBJECT: Wellington Street East Pedestrian Crossing Study – Preliminary Findings
OUR FILE: 17-5564

Introduction & Background:

The Town of Aurora has commissioned Dillon Consulting Limited (Dillon) to prepare a feasibility review involving identifying and evaluate options with supporting cost estimates for a multipurpose barrier-free trail crossing both legs of the Tim Jones Trail at Wellington Street East. The study area is in the vicinity of Wellington Street East (York Regional Road 15) and John West Way where two ends of the trail currently meet Wellington Street East adjacent to a watercourse (tributary to East Holland River) and culvert crossing, a stormwater management wet pond, Unevaluated Wetland (marsh), woodlands, existing underground and above ground utilities and is located within the regional floodplain.

The intent of this study is to identify a safe, appropriate and cost-effective crossing of Wellington Street to address the practice where some trail users traverse four lanes of regional road traffic and scale the median or utilize the official crossing at the available signalized crosswalk approximately 200m or 360m away (depending on travelling north or south) from where the existing path terminates at Wellington Street East.

The purpose of this memo is to provide a status update of the current findings of the work with a preliminary recommendation regarding the preferred crossing option and approximate order of magnitude costs.

Description of Options:

Dillon has prepared a long list of options and has assessed the advantages and disadvantages as well as developed preliminary drawings and costing for the primary/grade-change options (overpass and underpass).

Option 1 – Overpass Bridge (Above-grade crossing)

Option 1 involves an overpass, pile supported bridge structure consisting of a series of accessible ramps to raise trail users over the regional road. As per the Canadian Highway Bridge Design Code a minimum preferred vertical clearance of 5.25m is required for a pedestrian overpass structures. The preliminary layout of the bridge attempts to locate it entirely within the road right-of-way (ROW) so as to eliminate the need for the acquisition of additional lands. As such, this design implements a series of switch-backs, to accommodate the sloped distance and landings to conform to Accessibility Design Standards. One value-added benefit of the switch-backs is that it will prevent cyclists from quickly traversing and posing

safety concerns to pedestrian trail-users. However, traversing the series of switch backs on the either side of the crossing may discourage users; which may lead to low utilization of the crossing.

As per Dillon's discussions with the Lake Simcoe Region Conservation Authority, the bridge would have to be pile supported so as to minimize impact to the floodplain (which the crossing is currently within). The placement of the bridge's support system is to span the road and trigger little or no relocations to the existing subsurface utilities or creek culvert. During detail design, further field verification of utilities and culvert will be required to address and mitigate design conflicts. In addition to horizontal verification, a vertical clearance with the adjacent hydro (power transmission) poles should be field verified to clarify the need for relocation of electric services.

We feel that the bridge option could be constructed with minimal weekday traffic impact, with span construction taking place on off-peak/nights/weekend hours.

Option 2 – Underpass Tunnel (Below-grade crossing)

Option 2 is a 50m long precast underpass box-culvert structure to provide an efficient path connecting the two pathways with minimal grade change. The location was selected to avoid impacts to the existing creek culverts and neighboring stormwater management facility. The profile of the tunnel was determined to facilitate drainage northbound to direct runoff to the neighbouring creek. It is important to note that this underpass is within the floodplain and is expected to flood during periods of high rainfall. Currently this portion of Wellington Street East overtops during significant storm events. Just as in the bridge design, the tunnel is sloped to provide an accessible path and wide enough to support multiple trail users. The height of the tunnel was selected to provide a constructible structure that would reduce significant grading on both sides of the trail crossing, accommodate the existing grade of the regional road and allow a service/maintenance vehicle to enter. Given the confining nature of a tunnel, it is recommended that suitable lighting and Crime Prevent Through Environmental Design (CPTED) measures be adopted.

During detail design, further field verification regarding the serviceability, material, dimension and depth of subsurface utilities will be required so as to detail the relocation of the adjacent water, gas and sewer utilities.

Option 3 – Signalized mid-block intersection/pedestrian crossing (At-grade crossing)

Improvements to provide a formal, signalized mid-block intersection or provision of a pedestrian signal were considered involving push button and raised signage with flashing beacons that trail users would activate. However, the location of the mid-block crossing would be within 200m of the existing signalized intersection where Wellington Street East meets John West Way. The Region indicated that such a crossing was not warranted and this option was not pursued further.

Option 4 – Do nothing crossing

A do nothing option would likely involve the installation of barrier to prohibit crossing at-grade at the current location.

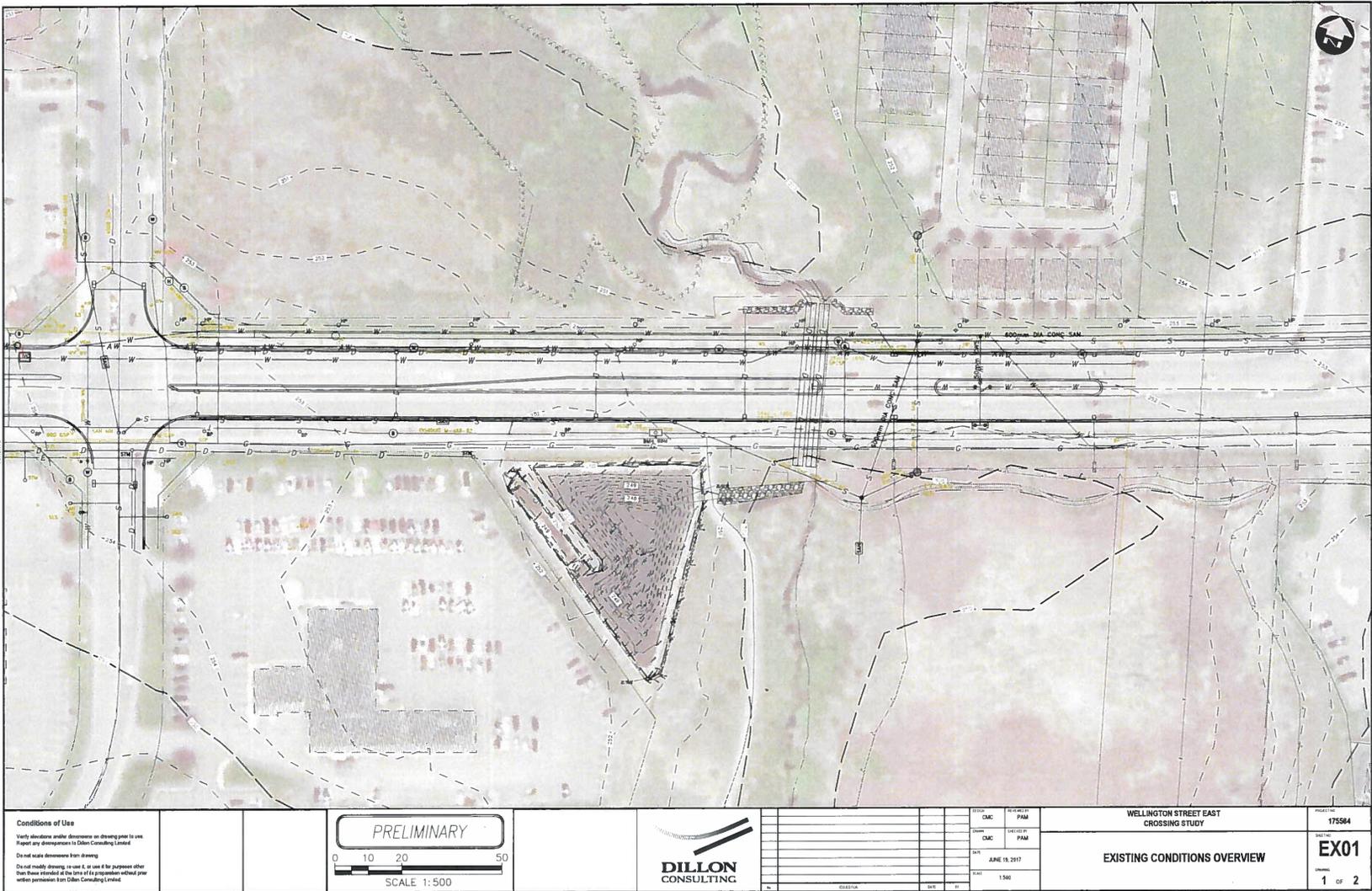
Preliminary Findings:

We attached the following support documents:

- Existing Conditions Overview Drawing (EX01)
- Concept 1: Above Grade Crossing – Site Plan and Profile drawing (SP01)
- Concept 2: Below Grade Crossing – Site Plan and Profile drawing (SP02)
- Costing for Concepts 1 & 2
- Overall concepts and analysis presentation

Concept 2 (underpass) is the recommended treatment subject to available funding. It will result in the most functionally superior solution, at less cost than its primary alternative (Concept 1 – Overpass). The approximate cost of the option is \$3.3 million. Concept 1; order of magnitude cost was \$10.3 million. Cost efficiencies may be achievable with the Concept 1 which could be investigated further in the design stage.

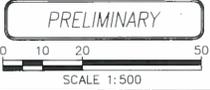
At a minimum we recommend examining the provision of barriers to prohibit path user from crossing Wellington Street East at this location.

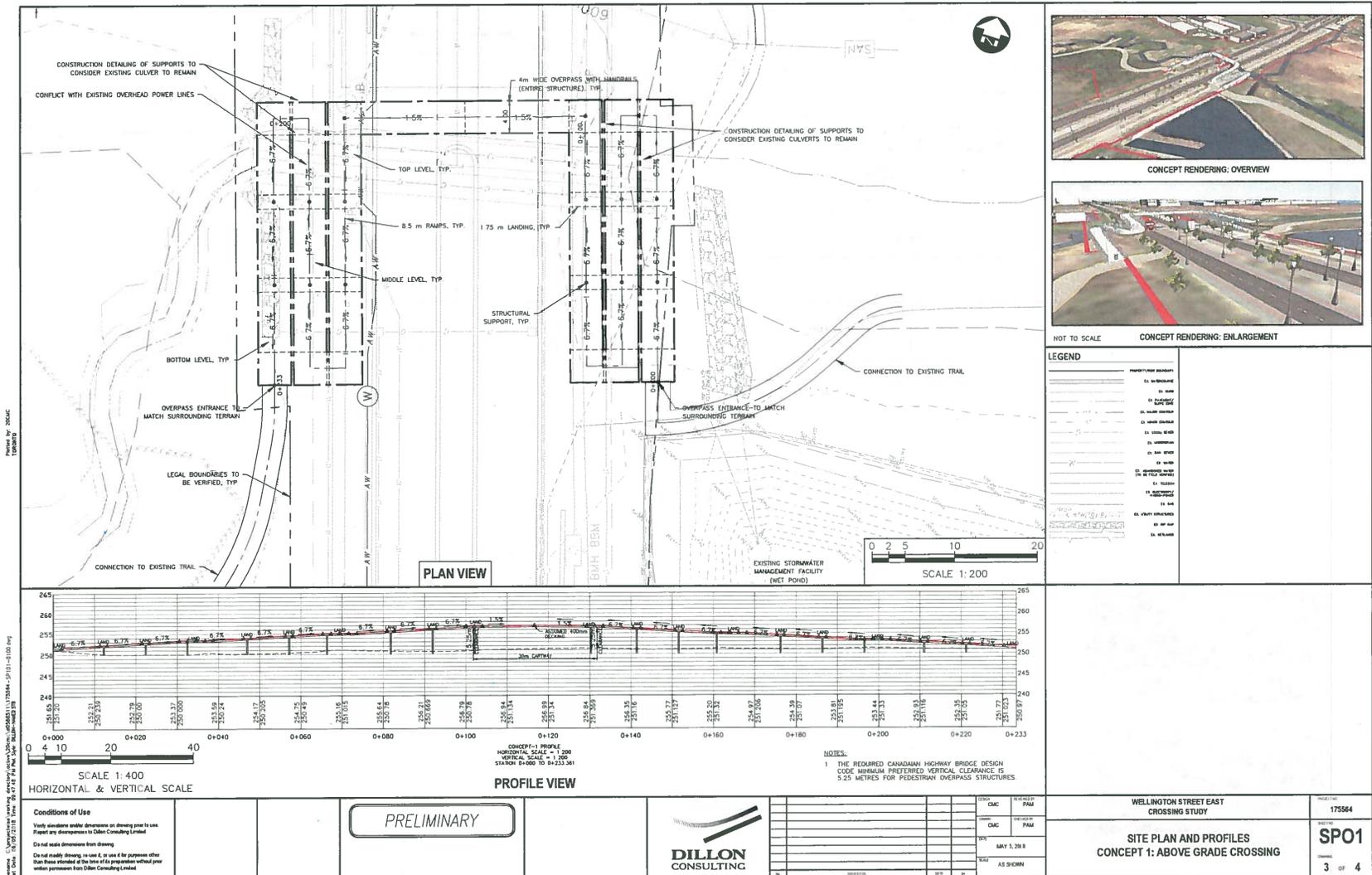


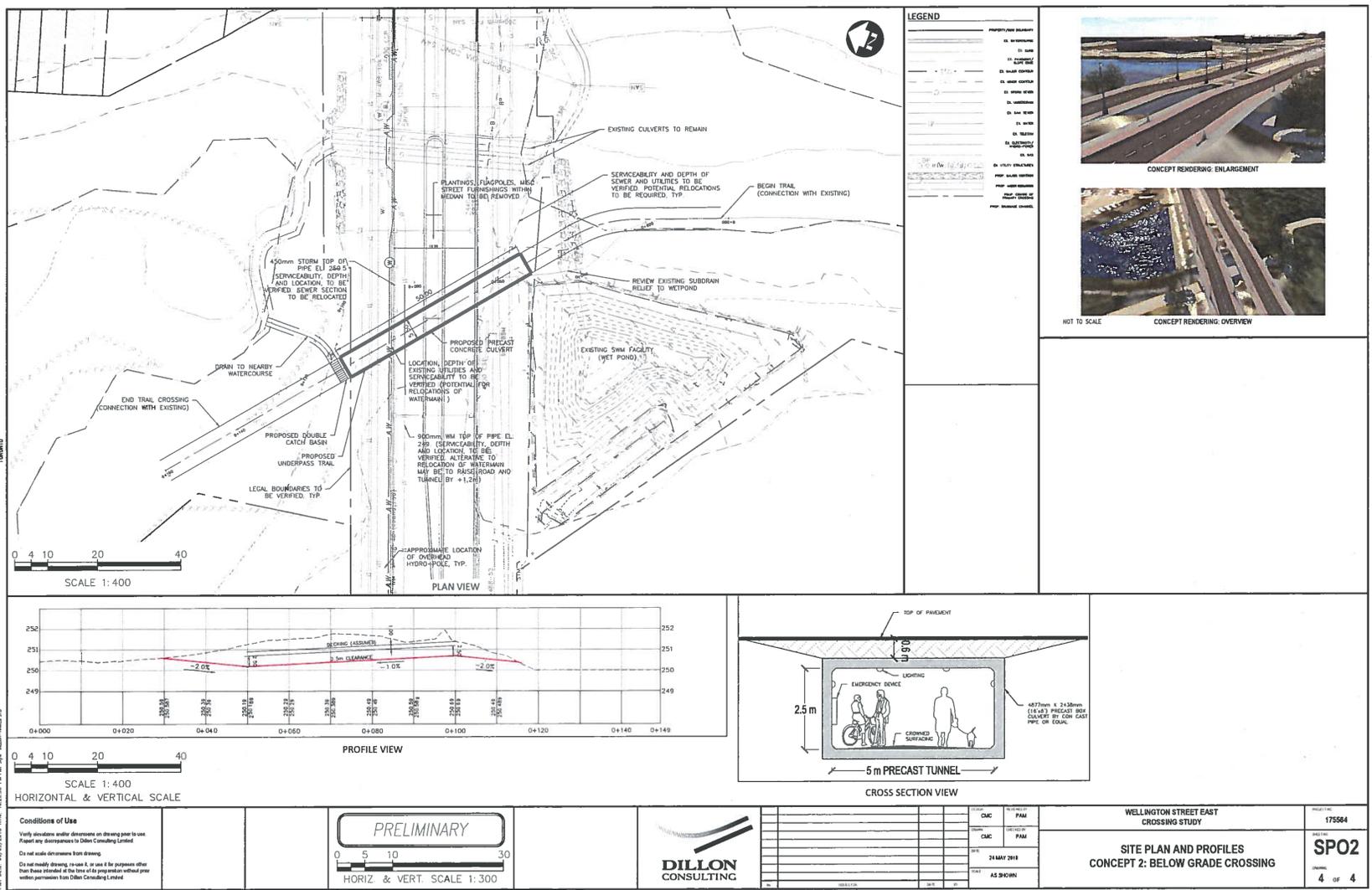
Prepared by: 20042C
 10/20/2016

Filename: C:\Projects\2016\Wellington Street East Crossing Study\Drawings\Wellington Street East Crossing Study.dwg
 Plot Date: 2018/06/15 10:00:00 AM
 Plot Scale: 1:500

Conditions of Use
 Verify dimensions and/or descriptions on drawing prior to use.
 Report any discrepancies to Dillon Consulting Limited.
 Do not scale dimensions from drawing.
 Do not modify drawing, re-use it, or use it for purposes other than those intended at the time of its preparation without prior written permission from Dillon Consulting Limited.







**Trails and Active Transportation Committee Meeting Agenda
Friday, June 15, 2018**

Town of Aurora
Wellington Street East Crossing Study

Costing review-Rev1 - Option 1 (Overpass)

Printed 2018-06-06

No.	Item	Unit	Estimated Quantity	Unit Price	Total
A PATHWAY & ROADWAY WORKS					
A1	Clearing and Grubbing/Site Prep	m ²	800.0	\$2.00	\$1,600.00
A2	150 mm Subdrain	m	600	\$30.00	\$18,000.00
A3	Entrance Modifications/Regrading	L.S.	1	\$5,000.00	\$5,000.00
A4	Earth Excavation	m ³	350	\$20.00	\$7,000.00
A5	Minor Works/Minor Items			15.00%	\$4,740.00
Subtotal Pathway and Roadworks					\$36,340.00
B STRUCTURAL COMPONENTS					
B1	Bridge overpass & foundations	L.S.	1	\$6,500,000.00	\$6,500,000.00
B2	Staging, Interim Traffic control, Temporary Works etc.	L.S.	1	\$100,000.00	\$100,000.00
Subtotal Structures					\$6,600,000.00
C LANDSCAPING					
C1	Plantings	L.S.	1	TBD	TBD
C2	Flagpole Removal	ea.	3	TBD	TBD
Subtotal Landscaping					TBD
D ELECTRICAL					
D1	Path and facade lighting	L.S.	1	TBD	TBD
D2	Hydro pole relocations	ea.	2	TBD	TBD
Subtotal Electrical					TBD
E UTILITIES & SERVICES					
E1	Utility Protection/Support/Relocation Allowance	L.S.	1		\$250,000.00
Subtotal Utilities					\$250,000.00
Subtotal All Items (excluding Property)					\$6,886,340.00
F CONTINGENCY					
F1	Contingency 30%				\$2,065,902.00
Subtotal All Items, including contingency					\$8,952,242.00
G ENGINEERING					
G1	Engineering (15%)				\$1,342,836.30
H TOTAL ESTIMATED COST (excluding HST)					\$10,295,078.30

Land costs, if any, are not included.

Town of Aurora
 Wellington Street East Crossing Study

Costing review-Rev1 - Option 2 (Underpass)

Printed 2018-06-06

No.	Item	Unit	Estimated Quantity	Unit Price	Total
A	PATHWAY & ROADWAY WORKS				
A1	Clearing and Grubbing/Site Prep	m ²	2500	\$2.00	\$5,000.00
A2	150 mm Subdrain	m	600	\$30.00	\$18,000.00
A3	Entrance Modifications/Regrading	L.S.	1	\$5,000.00	\$5,000.00
A4	Earth Excavation	m ³	1200	\$20.00	\$24,000.00
A5	Waterproofing	L.S.	1	\$15,000.00	\$15,000.00
A6	Sod c/w 100 mm Topsoil	m ²		\$8.00	\$0.00
A7	Double Catchbasin with outlet to watercourse	ea.	1	\$60,000.00	\$60,000.00
A8	Minor Works/Minor Items			15.00%	\$19,050.00
	Subtotal Pathway and Roadworks				\$146,050.00
B	STRUCTURAL COMPONENTS				
B1	Underpass (Rectangular box culvert)	m	50	\$30,000.00	\$1,500,000.00
B2	Underpass Wing Walls	m ²	150	\$1,500.00	\$225,000.00
B3	Approach slabs for box culvert	m ²	450	TBD	TBD
B4	Road decking replacement	L.S.	1	TBD	TBD
B5	Staging, Interim Traffic control, Temporary Works etc.	L.S.	1	\$250,000.00	\$250,000.00
	Subtotal Structures				\$1,975,000.00
C	LANDSCAPING				
C1	Plantings	L.S.	1	TBD	TBD
C2	Public Realm	L.S.	1	TBD	TBD
	Subtotal Landscaping				TBD
D	ELECTRICAL				
D1	Path Lighting	L.S.	1	TBD	TBD
D2	Emergency call device	L.S.	1	TBD	TBD
	Subtotal Electrical				TBD
E	UTILITIES & SERVICES				
E1	Water - Utility Protection/Support/Relocation Allowance	m	65	\$1,000.00	\$65,000.00
E2	Gas - Utility Protection/Support/Relocation Allowance	m	25	\$800.00	\$20,000.00
E3	Sanitary - Utility Protection/Support/Relocation Allowance	m	60	\$2,000.00	\$120,000.00
E4	Storm - Utility Protection/Support/Relocation Allowance	m	45	\$2,000.00	\$90,000.00
	Subtotal Utilities				\$90,000.00
	Subtotal All Items (excluding Property)				\$2,211,050.00
F	CONTINGENCY				
F1	Contingency 30%				\$663,315.00
	Subtotal All Items, including contingency				\$2,874,365.00
G	ENGINEERING				
G1	Engineering (15%)				\$431,154.75
H	TOTAL ESTIMATED COST (excluding HST)				\$3,305,519.75

Land costs, if any, are not included.

Existing Condition

(Intersection Crossing)



Concept-1

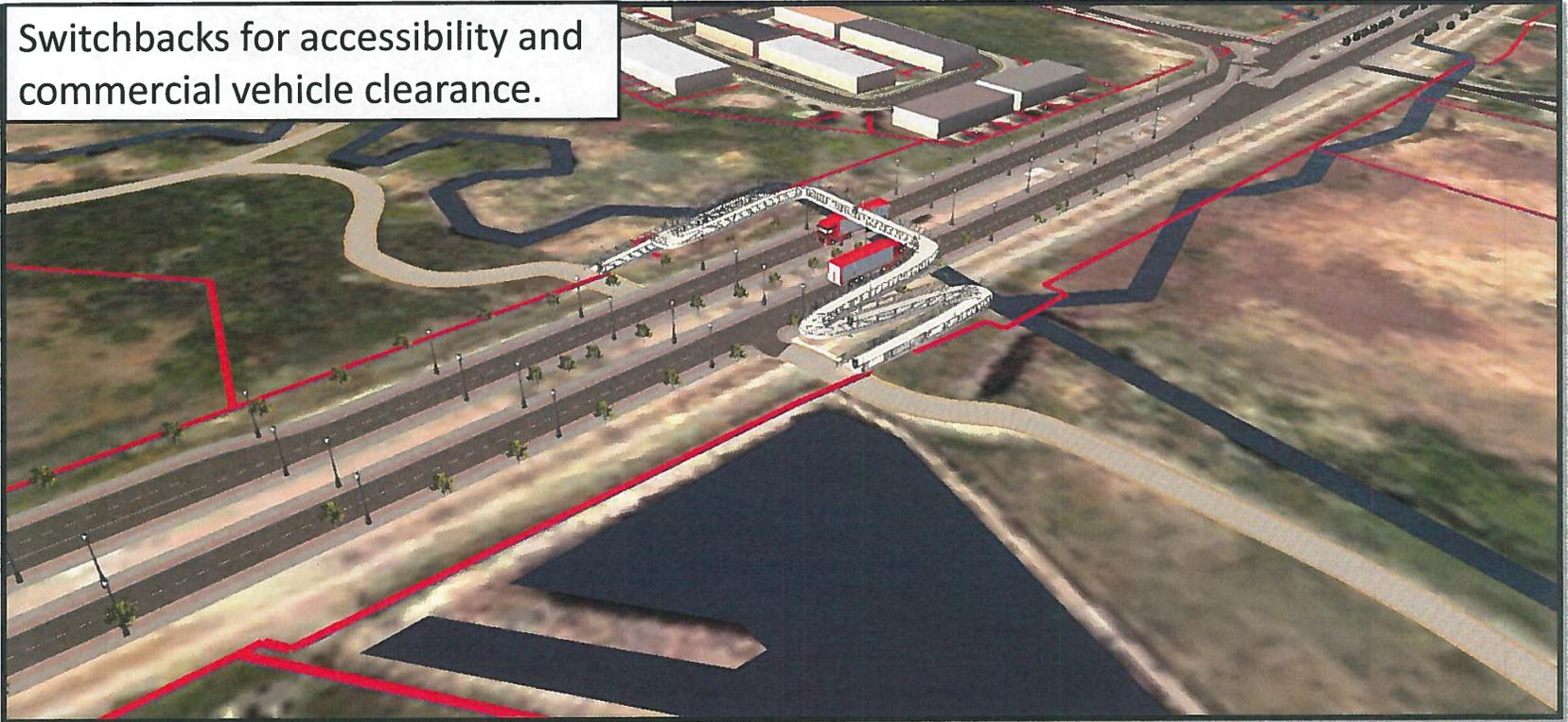
(Above grade Crossing)



Concept-1

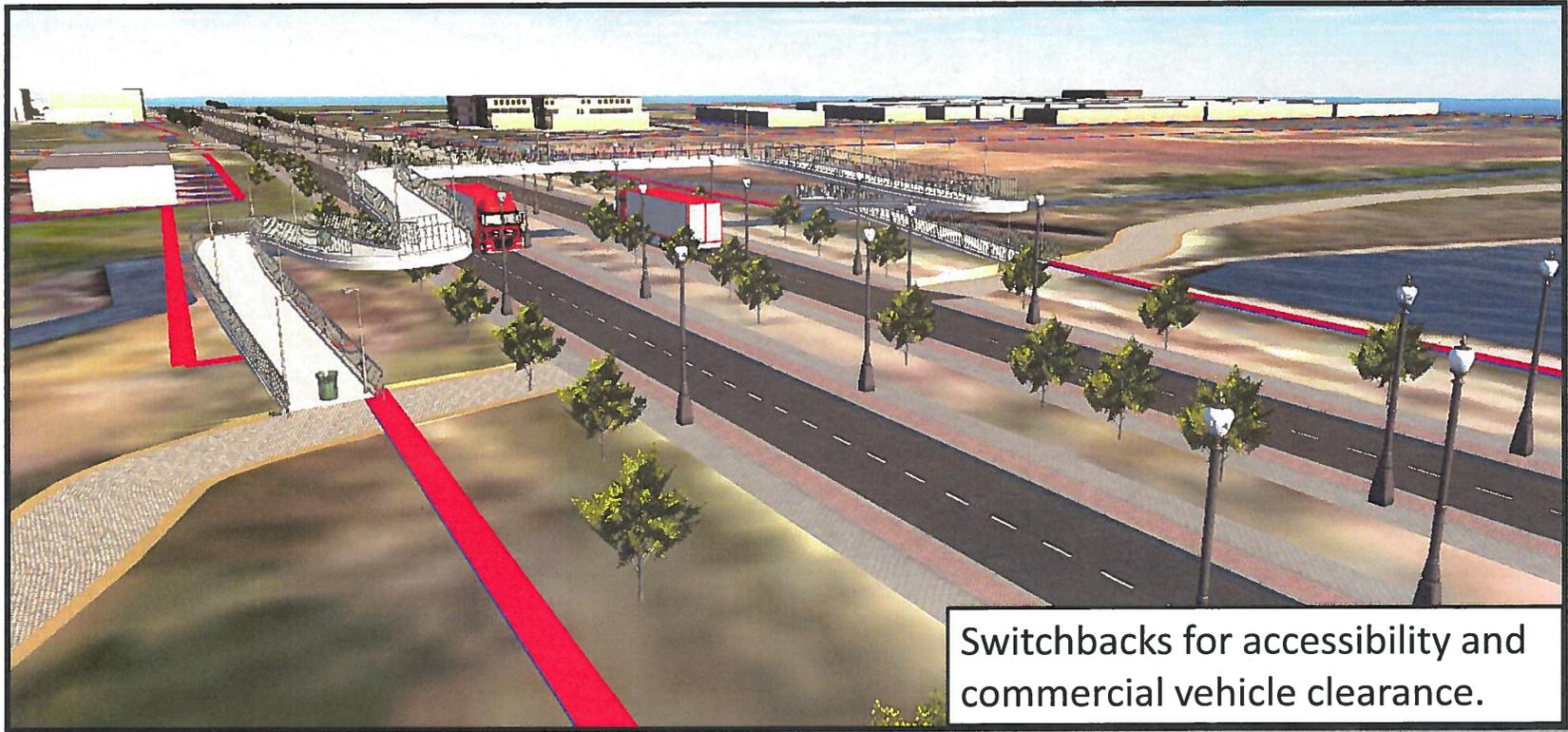
(Above grade Crossing)

Switchbacks for accessibility and commercial vehicle clearance.



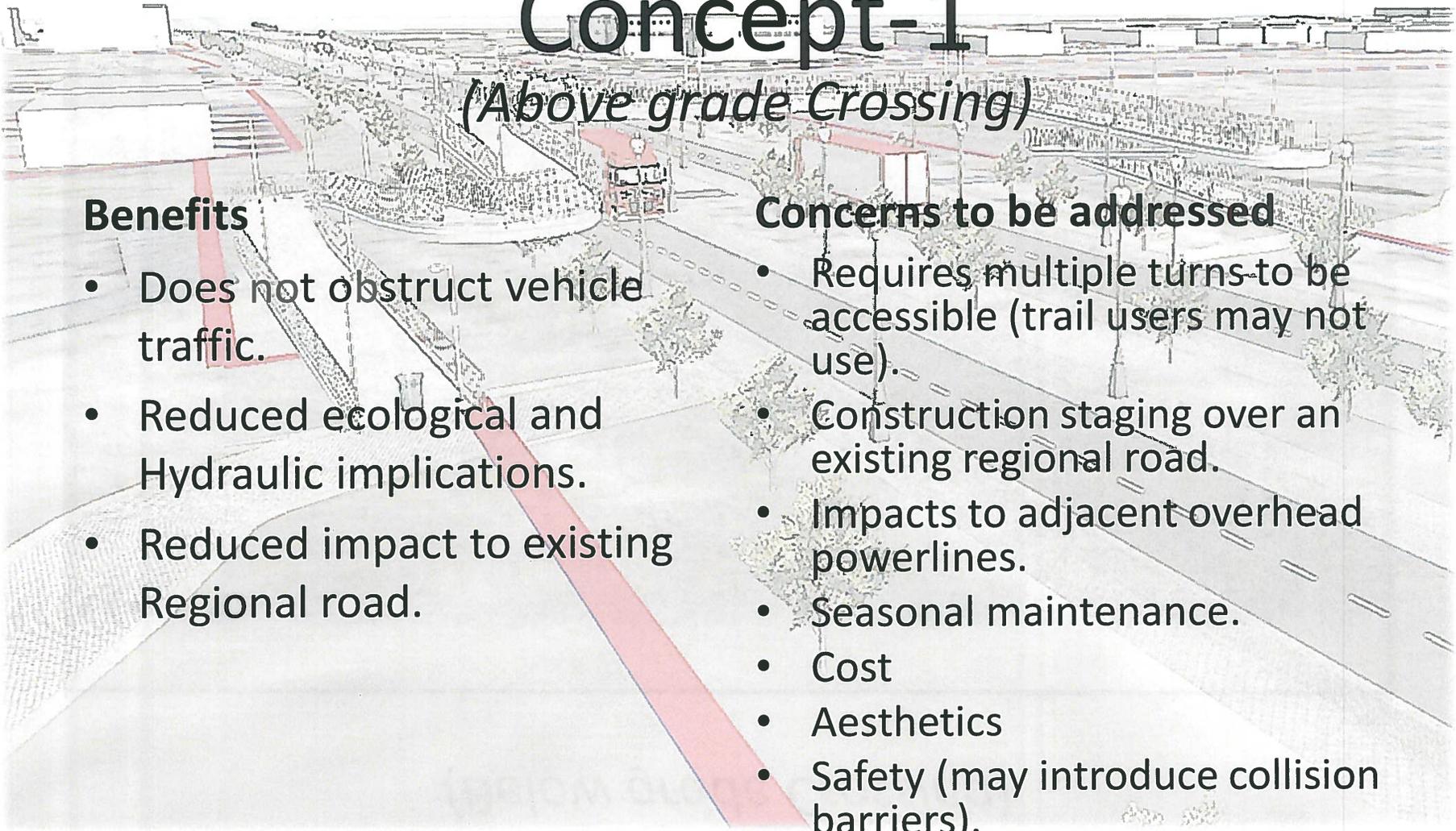
Concept-1

(Above grade Crossing)



Concept-1

(Above grade Crossing)



Benefits

- Does not obstruct vehicle traffic.
- Reduced ecological and Hydraulic implications.
- Reduced impact to existing Regional road.

Concerns to be addressed

- Requires multiple turns to be accessible (trail users may not use).
- Construction staging over an existing regional road.
- Impacts to adjacent overhead powerlines.
- Seasonal maintenance.
- Cost
- Aesthetics
- Safety (may introduce collision barriers).



Concept-2

(Below grade Crossing)



Concept-2

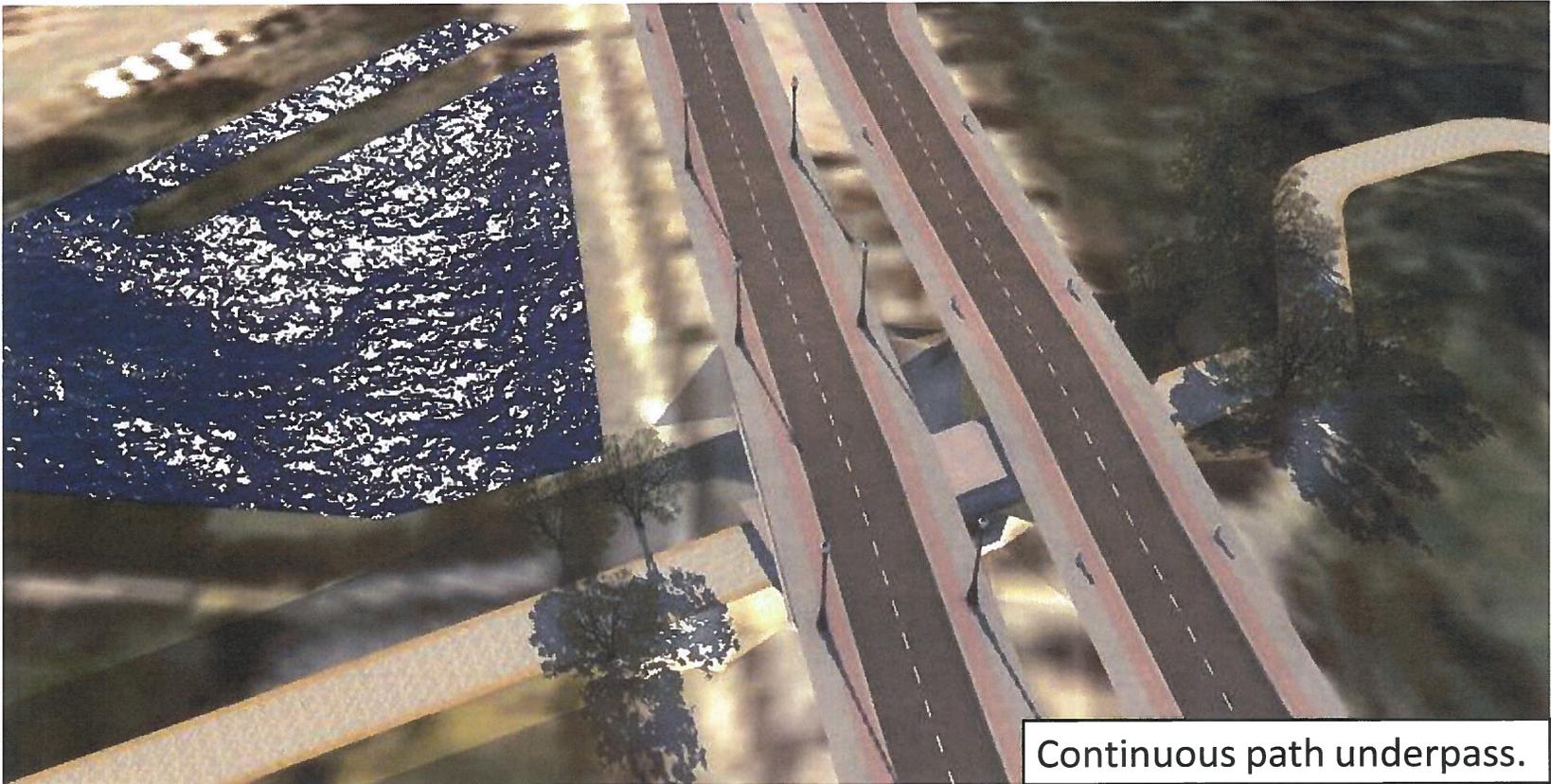
(Below grade Crossing)

Continuous path underpass.



Concept-2

(Below grade Crossing)



Continuous path underpass.



Concept-2

(Below grade Crossing)

Benefits

- Easily traversed path (high likelihood of use).
- Opportunity to incorporate with future commercial development.
- Avoids modifications to existing culvert

Concerns to be addressed

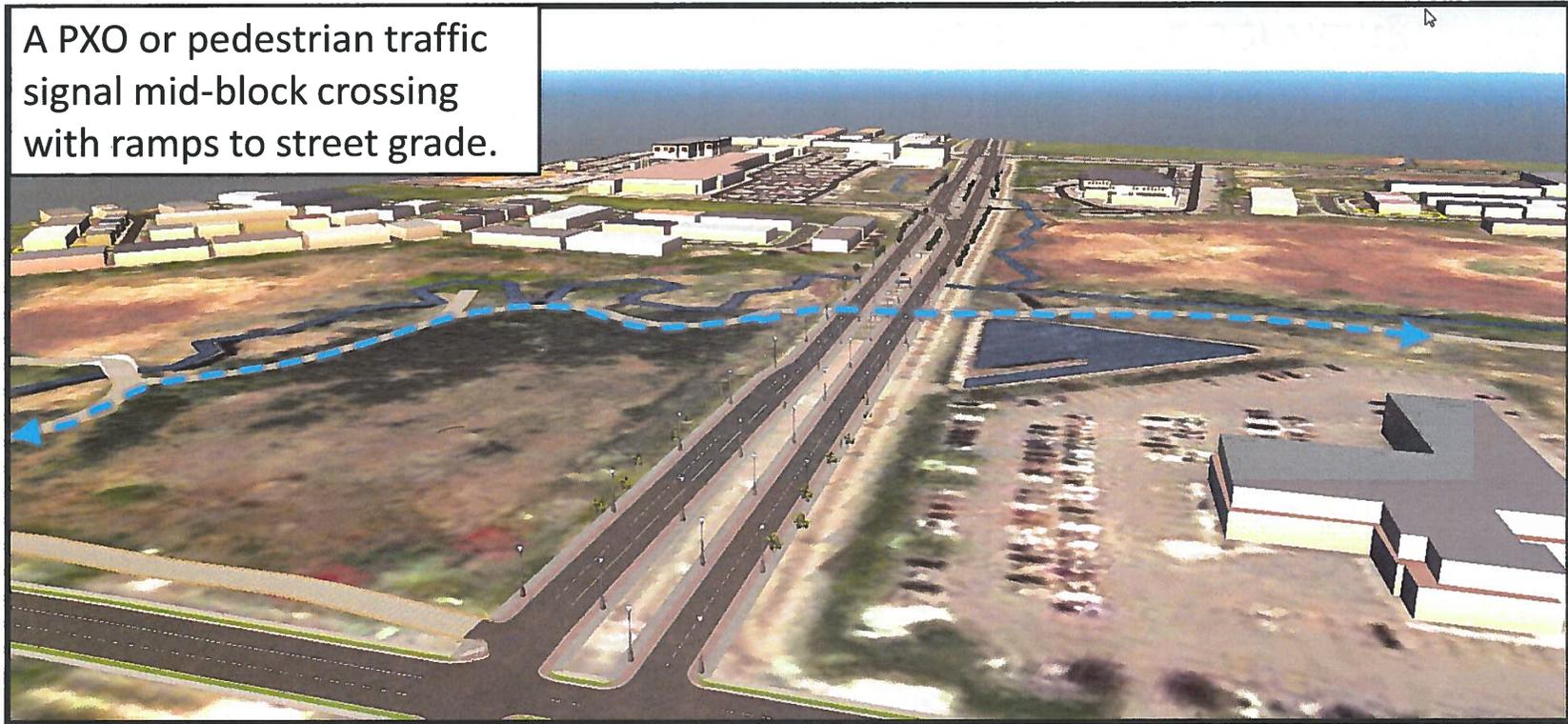
- Within the floodplain. Tunnel will flood during significant storm events.
- Traffic impacts during construction.
- Impacts to adjacent underground servicing.
- Lighting and safety need to be addressed.



Concept-3

(Signalized At grade Crossing)

A PXO or pedestrian traffic signal mid-block crossing with ramps to street grade.



Concept-3

(Signalized At grade Crossing)

Benefits

- Less impact and cost than Concepts 1 & 2.
- Trail use may coincide with low vehicular traffic periods.

Concerns to be addressed

- May cause traffic backup on regional road when in use.
- Requires more pedestrian confidence and warning than traditional PXO due to high vehicular speeds.
- Removal of existing flagpoles and modifications to the median

This concept was not accepted as it would not meet the Region's requirements for new pedestrian crossings or signalized intersections

Concept-4

(Remove crossing potential - barrier)

Strip trail, block access and vegetate.



Concept-4

(Remove crossing potential - barrier)

Benefits

- Cost effective way to eliminate crossings.
- Potential to free-up abandoned path lands.

Concerns to be addressed

- Reduced level of service to path users.



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**Town of Aurora
Operational Services
Parks Division**

Memorandum

Date: June 15, 2018
To: Trails and Active Transportation Committee
From: Gary Greidanus, Senior Landscape Architect
Re: **Trailhead Parking – 46 St. John’s Sideroad**

Recommendation

- 1. That the memorandum regarding Trailhead Parking – 46 St. John’s Sideroad be received for information.**

Background

Biddington Homes Aurora Inc. has applied to the Town of Aurora for Official Plan and Zoning By-law amendments in relation to a proposed townhouse development located at 46 St. John’s Sideroad. The development proposes 68 stacked townhouse dwellings in 3 blocks with access from St. John’s Sideroad. 96 parking spaces are proposed, both at grade and underground.

It is noted that this site is currently the location of informal parking by members of the public attending the adjacent boardwalk and trail system in the McKenzie Wetland.

Review comments made for the Official Plan and Rezoning applications identified the desire for public trailhead parking on this property and solicited a response from the Owner as to the feasibility of formal public parking. Biddington Homes, through their planning consultant, has responded that given the current proposal there is no space to offer public parking in addition to the parking required for this development.

A Planning report regarding the Biddington Official Plan and Rezoning is scheduled to go to the June 19th General Committee meeting but the report is silent on the issue of trailhead parking.

Trailhead Parking – 46 St. John’s Sideroad
June 15, 2018

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Attachments

Attachment 1 – Context Plan

Attachment 2 – Site Plan



CONTEXT PLAN	46 ST JOHN'S SIDEROAD TOWN OF AURORA	SCALE: NTS	DRAWN: CW	CHK'D: AM	GUTHRIE MUSCOVITCH ARCHITECTS Tel: (416) 252-5679 Fax: (416) 252-9637 770 Brown's Line, Toronto, Ontario M8W 3W2
			APRIL 24, 2017	PROJECT #: 16063	

